



Meeting Agenda: Thursday, April 6, 2023, 7:30 a.m.

City of Moscow Council Chambers • 206 E 3rd Street • Moscow, ID 83843
(A) = Board Action Item

1. **Consent Agenda (A)** - Any item will be removed from the consent agenda at the request of a member of the Board and that item will be considered separately later.
A. Minutes from March 16, 2023
ACTION: Approve the consent agenda or take such other action deemed appropriate.
2. **Public Comment**
Members of the public may speak to the Board regarding matters NOT on the Agenda nor currently pending before the Moscow Urban Renewal Agency. Please state your name and resident city for the record and limit your remarks to three minutes.
3. **Updated Calendar/Summer Schedule (Information Only) - Cody Riddle**
Staff will present a tentative schedule outlining key dates related to the development of the FY2024 Budget and review of proposals for the Sixth and Jackson property.
ACTION: Receive report and provide feedback as appropriate.
4. **Legacy Crossing Zoning and Design Standards (Information Only) – Cody Riddle**
Staff will provide an overview of the zoning and design standards applicable to development of the Sixth and Jackson property.
ACTION: Receive report and provide feedback as appropriate.
5. **General Agency Updates – Cody Riddle**
 - General Agency Business

NOTICE: It is the policy of the City of Moscow that all City-sponsored public meetings and events are accessible to all people. If you need assistance in participating in this meeting or event due to a disability under the ADA, please contact the City's ADA Coordinator by phone at (208) 883-7600, TDD (208) 883-7019, or by email at adacoordinator@ci.moscow.id.us at least 48 hours prior to the scheduled meeting or event to request an accommodation. The City of Moscow is committed to ensuring that all reasonable accommodation requests are fulfilled.



Meeting Minutes: March 16, 2023, 7:30 a.m.

City of Moscow Council Chambers • 206 E 3rd Street • Moscow, ID 83843

Commissioners Present	Commissioners Absent	Staff in Attendance
Steve McGeehan, Chair (virtual)	Mark Beauchamp	Cody Riddle, Executive Director
Maureen Laflin	Sandra Kelly	Jennifer Fleischman, Clerk
Alison Tompkins	Tom Lamar	Renee Tack, Treasurer
Nancy Tribble		

McGeehan called the meeting to order at 7:30 a.m.

1. Consent Agenda (A)

Any item will be removed from the consent agenda at the request of any member of the Board and that item will be considered separately later.

A. Minutes from March 2, 2023

B. February 2023 Payables

C. February 2023 Financials

Laflin moved for approval of the consent agenda as presented, seconded by Tompkins. Roll Call Vote; Ayes: Unanimous (4). Nays: None. Abstentions: None. Motion carried.

2. Public Comment

Members of the public may speak to the Board regarding matters NOT on the Agenda nor currently pending before the Moscow Urban Renewal Agency. Please state your name and resident city for the record and limit your remarks to three minutes.

None.

3. Agency FY2022 Audit Presentation (A) – Renee Tack

The 2022 MURA audit is attached and will be presented by MURA Treasurer Renee Tack and the auditors, Presnell Gage PLLC.

Riddle started the presentation by introducing Scott Dockins from Presnell Gage, who reviewed the process that the independent auditors use to compile the financial report and the various responsibilities of the City and the Agency. The resulting official opinion is that the URA's finances are in good standing.

Laflin moved to accept the FY2022 Audited Financial Statements as presented, seconded by Tribble. Tompkins commented that all the accounting procedures are in order and the Agency's finances are in good condition. Roll Call Vote; Ayes: Unanimous (4). Nays: None. Abstentions: None. Motion carried.

4. 2022 MURA Annual Report Review (A) – Cody Riddle

In accordance with State Statute, all urban renewal agencies are required to file with the local governing body by March 31st of each year an annual report describing the activities of the agency for the preceding year. Agencies are required to hold a public meeting to report the findings of the annual report and to take comments from the public prior to filing the report with the governing body. Staff has prepared the draft 2022 Annual Report for public comment and the Board's review and approval.

Riddle reviewed the draft Annual Report, as required by state law, and the public improvement projects that were completed or are pending contribution, in coordination with the City of Moscow. The design work for the South

Main Underpass project has started, which will look similar to the underpass that crosses Highway 8 on the east side of town. The Request for Proposals for the Sixth and Jackson property are due May 5th, 2023.

Public Hearing opened at 7:51 AM

Victoria Seever, Moscow, presented her written comment and talked about the importance of the Agency's work, and how it impacts the Moscow community. Various public improvement projects funded by the Agency were mentioned, as well as appreciation for that work.

Public Hearing closed at 7:56 AM

Laflin moved for approval of the FY2022 Annual Report and associated Resolution 2023-01, seconded by Tribble. Tompkins said that Annual Report was well done. Roll Call Vote; Ayes: Unanimous (4). Nays: None. Abstentions: None. Motion carried.

5. General Agency Updates – Cody Riddle

- *General Agency business*

None.

McGeehan declared the meeting adjourned at 7:58 a.m.

Steve McGeehan, Agency Chair

Date

2023-KEY UPCOMING DATES

May 5, 2023-Legacy Proposals Due

May 8, 2023-Legacy information distributed to Board and Subcommittee

May 18, 2023-Preliminary Discussion of Proposals with Board

May 19, 2023-Invitations to Present Issued

May 24-26, 2023-Subcommittee Meeting and Evaluation of Proposals

June 15, 2023-Review Draft Budget & CIP with Board

July 6, 2023-Legacy Presentations

July 10-14, 2023-Finance Committee Meeting on Draft Budget & CIP

July 20, 2023-Final Scoring of Legacy Proposals by Board

July 27, 2023-Budget & CIP Distributed to Board

August 3, 2023-Budget Hearing

RESOLUTION NO. 2015 - 12

A RESOLUTION OF THE CITY OF MOSCOW, IDAHO, A MUNICIPAL CORPORATION OF THE STATE OF IDAHO, AUTHORIZING THE AMENDMENT OF THE LEGACY CROSSING OVERLAY DISTRICT DESIGN GUIDELINES (DESIGN GUIDELINES) INCLUDED HERETO AND INCORPORATED HEREIN BY THIS REFERENCE; PROVIDING THAT HIS RESOLUTION SHALL BE IN EFFECT UPON ITS PASSAGE, APPROVAL, AND PUBLICATION ACCORDING TO LAW.

WHEREAS, the City of Moscow desires to shape and guide the redevelopment and transition of the historical industrial area within the City located between the University of Idaho Campus and the City's Central Business District through the Legacy Crossing Overlay District and associated Design Guidelines; and

WHEREAS, the City envisions the Legacy Crossing area redeveloping into a vibrant mixed-use development with limited surface parking and a variety of retail, office, and residential uses; and

WHEREAS, there are currently conflicting off-street parking requirements within the Legacy Crossing Design Guidelines and underlying Urban Mixed Commercial Zoning District; and

WHEREAS, the City wishes to amend the off-street parking requirements within the UMC Zone and Design Guidelines document to allow less surface parking and alleviate the conflicting requirements; and

WHEREAS, it is anticipated that uses other than retail, such as residential or office uses, may be developed on ground floors and the current window area requirement may present a challenge to those uses; and

WHEREAS, the City wishes to amend the current window area requirement to allow for greater separation and privacy for uses other than retail uses that are located on the ground floor; and

WHEREAS, the Moscow Planning and Zoning Commission conducted a public hearing on the proposed amendments to the Legacy Crossing Design Guidelines on May 13th, 2015 at which time the Commission recommended unanimous approval of the amendments to City Council; and

WHEREAS, the Moscow City Council conducted a public hearing on the proposed amendments to the Legacy Crossing Design Guidelines on July 6th, 2015.

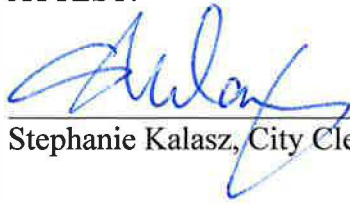
NOW, THEREFORE, BE IT RESOLVED by the Mayor and City Council of the City of Moscow as follows:

1. The attached Legacy Crossing Overlay District Design Guidelines is hereby adopted and shall be effective upon publication according to law.


2. That this Resolution shall become effective as of the 6th day of July, 2015.

PASSED AND APPROVED by the Mayor of the City of Moscow, Idaho, this 6th day of July, 2015.

ATTEST:


Stephanie Kalasz, City Clerk




Bill Lambert, Mayor



Legacy Crossing Overlay District Design Guidelines

Adopted June 4, 2012
Amended July 6, 2015





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Introduction

A. Community Vision

Throughout the fall of 2007 and spring of 2008, University of Idaho architecture students, property and business owners, residents, and elected officials engaged in a series of collaborative charettes and public meetings to develop a community-based vision for the future of the Legacy Crossing Urban Renewal District.

The vision captured the community's desire to bridge the divide between the University of Idaho Campus and the Downtown and energize and revitalize the core of the community. Throughout the process it was clear that the community envisioned the development of this area with high intensity dynamic and vibrant mixed uses.

Example illustrations of this vision are shown in Figure 1.1. These design guidelines were developed to facilitate the implementation of the vision.

B. Subject Area

The Legacy Crossing Overlay District (LCO) is that area (shown in Figure 1.2, and illustrated in more detail in Appendix A) generally bounded by Sixth Street to the north, Sweet Avenue to the west, Jackson Street to the east, and State Highway 95 to the south. This



Figure 1.1 - Conceptual Development Illustrations



area is generally located between the University of Idaho campus and the central business district and which has been historically utilized for agricultural, industrial and rail transportation uses.



Figure 1.2 - Subject Area

C. Purpose and Intent

In consideration of the unique characteristics of the LCO area, the redevelopment of this area requires an increased level of attention and consideration of site ingress and egress; building architectural style, placement and massing; public streetscape design and function; internal vehicular and pedestrian circulation; parking provision and management; preservation and enhancement of riparian areas; and the integration of public community spaces, public art and other public enhancements.

These development guidelines are intended to complement the City's Zoning Code by establishing development standards and guidelines for developments within the LCO. These guidelines are to be utilized by City Staff, the Planning and Zoning Commission, and City Council in their review of all developments within the LCO area.

The items included within this document are intended to be guidelines, and as such, minor deviations or variations may be allowed by the reviewing body where justified by the specific limitations or constraints of the development site, project layout and where such variations and deviations do not conflict with the intents and purposes of these Design Guidelines.



D. Land Uses

The redevelopment of the LCO area is generally anticipated to include a wide range of mixed-uses including retail, artists studios, eating and drinking establishments, limited light manufacturing where appropriate, professional office space as well as residential dwelling units. These design guidelines are generally intended to address the physical development of the LCO area rather than specific uses which are to be guided by the underlying zoning of the LCO area.



External Site Ingress & Egress

2

A. Introduction

This section addresses the exterior boundaries of the site and their relationship with the adjacent city street system as it pertains to logical and desirable ingress and egress points upon adjacent streets. As the site is principally bounded by a national highway (U.S. HWY 95) and a collector street that serves as one of the principal transportation corridors between the University of Idaho and the downtown and residential areas to the east (Sixth Street), planning for the safe and efficient ingress and egress to and from the site is critical to the successful redevelopment of the area and the safe function of the adjacent streets.

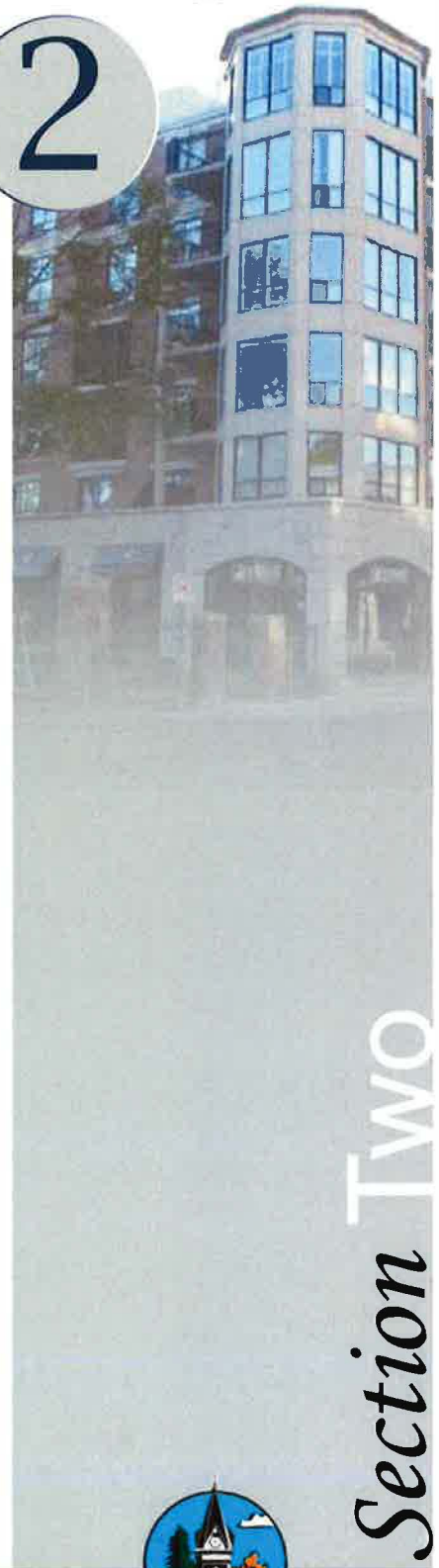
B. Surrounding City Streets

The site is generally bounded by Sixth Street to the north, Jackson (U.S. Highway 95) to the east, and Sweet Avenue to the south. The site is also bisected by College Street through the center of the LCO area.

Sixth Street adjacent to the LCO area is a designated collector street with widened urban sidewalks near newer developments and structures primarily located at or very near the property lines.

Jackson Street adjacent to the subject property is a three-lane street section that serves as the southbound segment of U.S. Highway 95 through the City. Sidewalks along Jackson are present in limited locations and are of insufficient width to meet an urban walk standard. Uses adjacent to Jackson are primarily the few remaining agricultural/industrial uses and structures, with the exception of the area near the Sixth Street intersection.

College Street bisects the LCO area and in many areas lacks any curbing, sidewalks and on-street parking facilities. Adjacent uses and structures are primarily agricultural/industrial in nature or undeveloped lands.



Section Two





Figure 2.1 - External Site Access Points

C. Site Access Locations

Principal vehicular access points to the site should be located so as to permit smooth traffic flow in and out of the site that minimize hazards to vehicular or pedestrian traffic upon adjacent public streets, pathways and sidewalks. As such, developments within the LCO area should incorporate the following features:

2.C.1: Principle access points to the development site should be located opposite existing street alignments and intersections.

2.C.2: The number of access points upon adjacent streets should be limited to minimize hazards and promote logical and orderly shared access and internal circulation within the LCO.

2.C.3: The preferred principal access locations are illustrated in Figure 2.1.



Internal Site Access and Circulation

3

A. Introduction

To facilitate the orderly development of the LCO it is necessary to ensure that there is adequate internal circulation and access to allow for the full development of the site. In consideration of the configuration of property ownership and the physical dimensions of the site, it is clear that internal circulation and access would be enhanced with the extension of a street through the center of the site as well as a secondary access point via Jackson Street as illustrated in Figure 4 and Appendix A.

These public streets will provide for the creation of significant commercial frontage that currently does not exist as well as provide access to customers, emergency service providers, public utilities, service and delivery, as well as the provision of public on-street parking to serve adjacent land uses.

B. Legacy Avenue

The existing property boundary configuration which reflects the historical railroad corridors does not facilitate the logical and orderly development of the LCO area, nor does it provide for efficient circulation and service delivery. Many properties within the LCO area are long and narrow, lack street frontage and access, and have no block structure to build upon. To address this condition, it is proposed that a public street be developed through the center of the LCO area on an alignment that roughly parallels Paradise Creek. In addition to providing the principal access to the site and the adjacent developable properties, this public street corridor will create significant commercial frontage and the opportunity to provide public parking and other amenities to serve development within the area.



Figure 3.1 - Internal Site Access and Circulation



Primary access to this new roadway, referred hereinafter as Legacy Avenue, would be gained from College Street and extend northwesterly and southeasterly from its intersection with College Street. The City is in the process of securing the majority of the required right-of-way for the extension of Legacy Avenue north of College Street. It is envisioned that the northwesterly extension of Legacy Avenue may terminate temporarily, or permanently, in a traffic circle south of current Taco Time Restaurant, or it may continue northwesterly to an intersection of Sixth Street in some configuration as development or redevelopment occurs within the area.

The southeasterly extension of Legacy Avenue is envisioned to extend from College Street and then turn northeasterly to connect to Jackson Street. This connection from College Street to Jackson Street would provide multiple points of access and enhanced circulation for vehicle access and emergency service provision to the three larger parcels within the area south of College Street. It is possible that access and cross circulation could be provided in the form of private roadways and accompanying public access easements that provide the equivalent functional access, circulation, and development form, depending upon the proposed development and subject to the approval of the reviewing body.

It is anticipated that public right-of-way necessary to accommodate Legacy Avenue would range from 70 to 85 feet in width depending upon on-street parking configuration as illustrated in the proposed street sections within Section 4.

3.B.1: All developments within the LCO should incorporate the extension of Legacy Avenue in conformance with the approved street sections described in these guidelines.

C. Jackson Street Connection

While Legacy Avenue provides for the internal access and circulation, the Jackson Street Connection into the site provides for a consolidated shared access to Jackson Street which serves as the southbound half of the U.S. HWY 95 couplet that encompasses the downtown area.

While the LCO area has a significant length of frontage along Jackson Street, due to its function as U.S. Highway 95 and the Idaho Transportation Department's access management program, access to the LCO site via Jackson Street is limited to ensure that entrance and exit turning movements do not jeopardize the safety and traffic carrying capacity of the adjacent highway. Therefore, the management and provision of a consolidated shared access to Jackson Street via



the Legacy/Jackson Connector is necessary.

3.C.1: All developments within the LCO should incorporate the Jackson Street Connection to intersect with Legacy Avenue to serve as a consolidated point of access to Jackson/U.S. Highway 95.

D. Loading Zones, Service Areas, and Refuse Management

3.D.1: Where feasible, rear service and loading areas should be provided, via public alley or private drive access, to direct such activities away from the public streets and commercial frontages.

3.D.2: Dumpsters and similar trash and recycling receptacles shall be located and physically screened from view from adjacent public streets and walkways.



Streetscape Design Guidelines

4

A. Introduction

Streets and sidewalks are key to the livability, vitality and identity of urban areas. Well designed streets and sidewalks add value and act as a catalyst to the development of private property. They provide the setting for land use, and the capacity of the street and sidewalk systems to carry vehicular and pedestrian traffic determines, in part, the appropriate adjacent land uses. Sidewalk width, street lights and other amenities affect pedestrian activity and aesthetic quality and thus help to determine how adjoining private land may be developed and used.

B. Urban Street and Sidewalk Standard

Urban sidewalks are generally located in business and mixed-use districts where there is a concentration of higher intensity development, ground floor retail, restaurants and entertainment uses, and on-street parking. They are designed for heavy pedestrian use and to be compatible with uses in a dense urban environment.

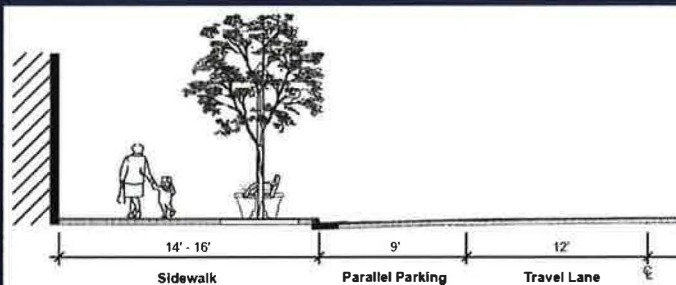
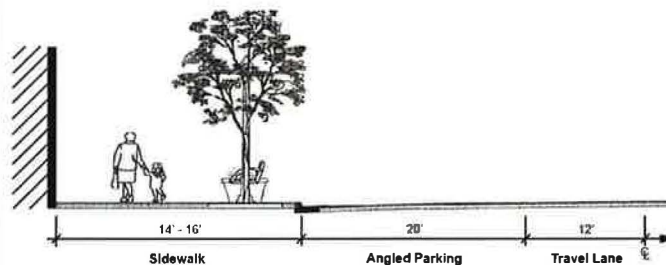
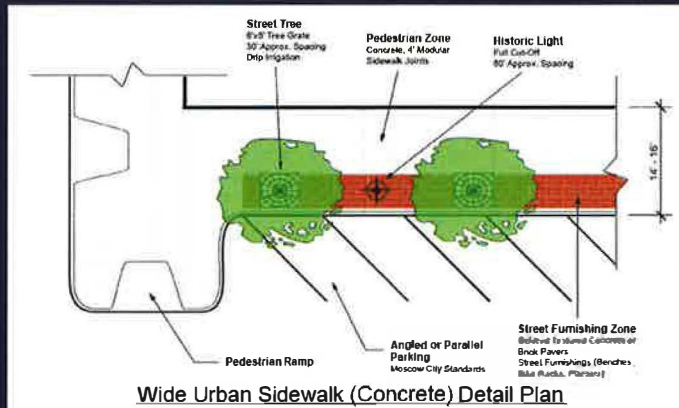


Figure 4.1 - Urban Sidewalk Standard Details



C. Street Design Guidelines

General Standards:

4.B.1: All public street frontages within the LCO area should be developed to a wide urban sidewalk including the provision of a minimum five (5') foot wide furnishing zone and minimum nine (9') foot wide pedestrian zone as illustrated in Figure 4.1 and Appendix A.

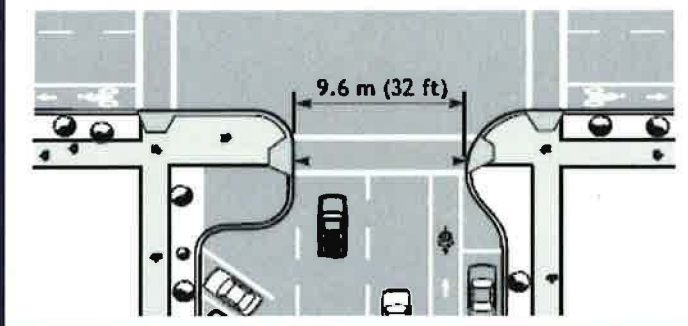


Figure 4.2 - Example Pedestrian Curb Extensions

4.B.2: The furnishing zone should include a minimum of five (5') foot diameter street tree wells with drip irrigation spaced approximately thirty (30) feet on center and full cut-off street light fixtures approximately sixty (60) feet on center, or as otherwise specified.

4.B.3: All public streets within the LCO should include on-street vehicular parking when possible via angled or parallel parking stalls.

4.B.4: All intersections and pedestrian crossings should include accessible pedestrian curb extensions (bump-outs) to promote pedestrian use and improve pedestrian safety as illustrated in Figure 4.2.

Legacy Avenue:

Legacy Avenue is anticipated to serve as the primary pedestrian and vehicular internal access and circulation corridor through the LCO area, but will also serve as the primary public parking area to serve development in the LCO area.

Public parking provides for the greatest flexibility in parking utilization and if dispersed throughout the LCO area along the Legacy Avenue alignment, will provide conveniently located parking throughout the LCO area that will reduce the amount of parking that must be privately provided, thus increasing the developable area within the LCO area.

4.B.5: Developments adjacent to Legacy Avenue should include street improvements in accordance with Figure 4.1.



Sixth Street:

Sixth Street borders the LCO site to the north. Recent development along Sixth Street (University Pointe) has included the development of a wider urban sidewalk and on-street parallel parking.

4.B.6: Developments adjacent to Sixth Street should include street improvements in accordance with Figure 4.1.

Jackson Street Connection:

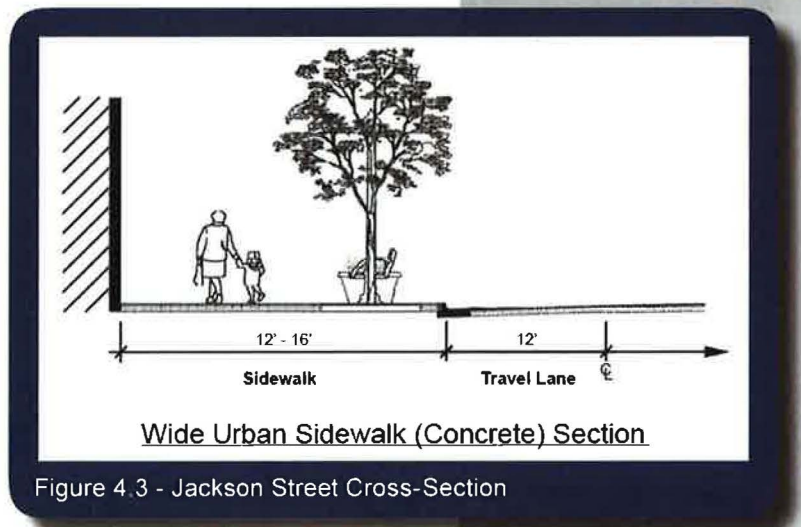
The Jackson Street Connection is anticipated to serve a similar function and be developed to a similar standard as Legacy Avenue.

4.B.7: All development within the LCO area should include the construction of the Jackson Street Connection in accordance with the proposed street design which includes a 12 to 16 foot wide urban sidewalk area, on-street parking areas as well as the vehicular travel lanes as shown in proposed street cross-section of Figure 4.1 and Appendix A.

College Street:

College Street bisects the LCO site and should be developed in a similar fashion as that of the Legacy Avenue and Jackson Street Connection with angled parking bays and wider urban sidewalks as shown in the proposed street cross-sections.

4.B.8: Development adjacent to College Street should include street improvements in accordance with Figure 4.1.



Jackson Street:

Jackson Street adjacent to the LCO area is a three-lane, one-way street which currently serves as the southbound half of the U.S. HWY 95 corridor through the downtown area. Ultimately it is anticipated that at some future date U.S. 95 may be rerouted outside of the downtown area which will allow for the future reconfiguration of the street. In the interim, it is anticipated that the Jackson Street frontage will primarily include the installation of a street furnishing/tree zone as



Section Four

well as a wider urban sidewalk as shown upon the proposed street cross-section shown in Figure 4.3.

4.B.9: Developments adjacent to Jackson Street should include street improvements in accordance with Figure 4.3.



Pedestrian Connectivity

5

A. Introduction

Due to the Redevelopment District's unique location between the University of Idaho Campus and the Central Business District, pedestrian access to and through the Redevelopment District is an important consideration. Developments proposed within the LCO should provide pedestrian facilities in accordance with the provisions of this Section.

B. Principal Pedestrian Corridors

5.B.1: All development proposals should incorporate and provide for the three pedestrian ways illustrated in Figure 5.1 and Appendix B including:

1. The continuation of the "Hello-Walk" which extends from the southwest corner of the intersection of Sixth and Jackson Streets southwest to a location near the rear of the University of Idaho Bookstore facility in accordance with Figure 5.2;
2. The extension of the current Sweet Avenue parking lot central walkway in accordance with Figure 5.3; and
3. The historic Main Street alignment and bridge structure across the southeast corner of the LCO Area in accordance with Figure 5.3.

C. Pedestrian Corridor Facilities

All pedestrian corridor facilities should be developed with the following design features and elements specified below:

5.C.1: All pedestrian corridor facilities should be surfaced with permanent hard surface of concrete unit pavers, concrete with



Figure 5.1 - Critical Pedestrian Corridors



special texture colors and patterns, brick or other paver blocks, or other similar materials as illustrated in Figure 5.2 and Figure 5.3.



Figure 5.2 - Hello-Walk Corridor Detail

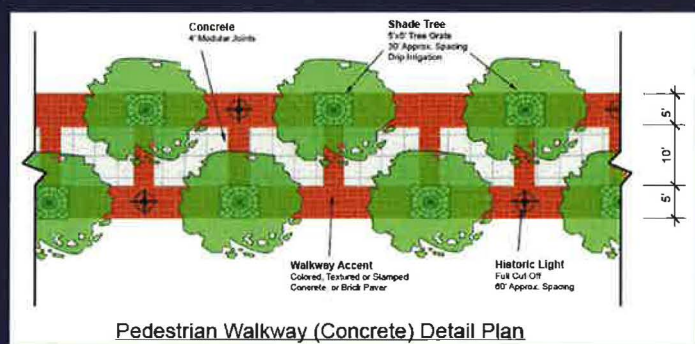


Figure 5.3 - Pedestrian Corridor Detail

5.C.2: All pedestrian corridor facilities should incorporate canopy trees and other vegetative plantings with drip irrigation to provide shade and shelter for pedestrians as illustrated in Figure 5.2 and Appendix B.

5.C.3: All pedestrian corridor facilities should include the integration of street furnishings (trash receptacles, benches, bollards, street lighting fixtures, bike racks, and other similar furnishings) as illustrated in Figure 5.2 and Appendix B.

5.C.4: All pedestrian corridor facilities should incorporate public art through surfacing patterns or designs, sculpture installation, and other means.



Figure 5.4 - Pedestrian Corridor Example



Building Design Guidelines

6

A. Introduction

A significant component to the ultimate realization of the community's vision for the development of the LCO area as a vibrant mixed-use pedestrian oriented urban center is the placement and character of the buildings that are constructed within the area. This includes building placement, height and architectural features (building materials and design) that together, with the other elements addressed in these guidelines, will assist in achieving the desired character of development in the LCO Area.

B. Building Placement

In order to ensure that the development within the LCO complements the urban character of downtown, all structures should be located upon the property in accordance to the following guidelines:

6.B.1: All buildings constructed adjacent to any public street (specifically Jackson/U.S. HWY 95, Sixth Street, Legacy Avenue, and the Jackson Street Connection) within the site should be located within five (5) feet of the front and/or street side property lines as illustrated in Figure 6.1.

6.B.2: Portions of buildings may be setback a maximum of fifteen (15) feet from the build-to-lines for no more than twenty (20%) percent of the total building frontage length to allow for façade articulation and the creation of public space amenities that are deemed by the hearing body to provide public benefit.

6.B.3: Minor façade articulations of less than three feet in depth should be considered as being placed upon the build-to-line.

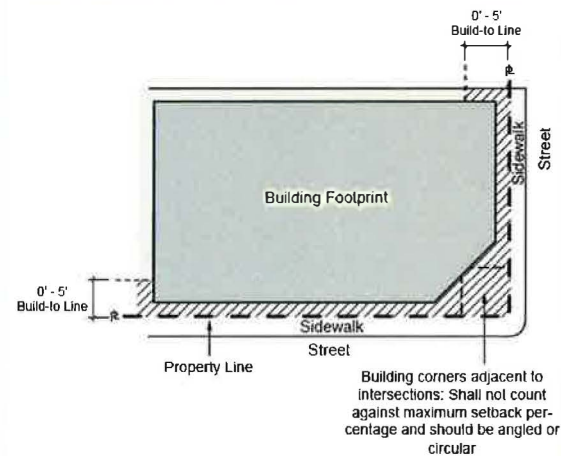


Figure 6.1 - Building Placement Diagram





Figure 6.2 - Example Corner Treatment



Figure 6.3 - Example Desired Building Height and Step Back

6.B.4: Building setbacks from the build-to-line at intersection corners that are necessary to preserve visual sight distances are not to be counted against the maximum setback percentage and should be accomplished via angled or other building corner treatments to provide adequate motorist and pedestrian visibility as illustrated in Figure 6.2.

C. Building Height

6.C.1: The minimum building height within the LCO should be twenty (20') feet or a minimum of two (2) stories. If an applicant proposes to construct a single story structure, the building parapet wall heights shall be increased to provide massing and appearance similar to that of a two story structure.

6.C.2: The maximum height at the build-to-line should be sixty (60) feet or five (5) stories. Any portion of a structure above such height should be stepped-back fifteen (15) feet from any property line adjacent to a public street or pedestrian corridor to ensure that the structure retains a pedestrian scale as illustrated in Figure 6.3.

6.C.3: The approving body may allow deviations from the minimum and maximum building heights if deemed justified by the character or nature of the proposed development.



D. Building Design

General Building

Façade Design:

Building façade design should comply with the following guidelines:

6.D.1: The front facade of all buildings should include the main entrance to the building and a number of transparent window openings.

6.D.2: Blank walls on all facades that front a park, street, plaza, or other public spaces should be discouraged.

6.D.3: On all facades, a clear visual distinction between each floor should be provided.

6.D.4: Facades should include both solid surfaces and window openings to avoid the creation of blank walls and/or glass curtain walls. Window openings should be balanced to avoid large blank wall surfaces on the façade.

6.D.5: Rear and side facades should be designed with similar architectural elements, materials, and colors as the front façade. However, the design of side and rear facades may be simpler, and more casual in nature.

Window Openings:

Window openings should be designed as follows:

6.D.7: Fifty (50%) to eighty-five (85%) percent of ground-floor



Figure 6.4 - Projecting Façade Element Example



façades of retail uses that front public streets and pedestrian ways should be occupied by windows and/or entry doors. The ground floor façades of all other uses that front public streets and pedestrian ways may have less than fifty (50%) percent window and/or entry door area when not located along primary activity centers in the LCO area. However, such ground-floor façades should not have less than thirty-five (35%) percent occupied by windows and/or entry doors.

6.D.8: Windows and doors should utilize clear transparent glass in order to provide clear views of storefront displays from the street and to allow natural surveillance of the street and adjacent outdoor spaces.

6.D.9: Mirror and tinted glass should be discouraged. Moderately tinted glass for energy conservation should be allowed.

6.D.10: For all floors above the ground floor, windows should occupy at least twenty (20%) of the total wall areas and should have a vertical orientation and proportion.

Projecting Façade Elements:

Projecting façade elements that provide shade and shelter to pedestrians are encouraged as follows:

6.D.11: Projecting façade elements should be in keeping with the historic architecture within the community including awnings and marquees.

6.D.12: Where buildings are adjacent to public streets or pedestrian ways, at least 75 percent of the façade width should contain projecting façade elements that provide shade and shelter to pedestrians as illustrated in Figure 6.4.

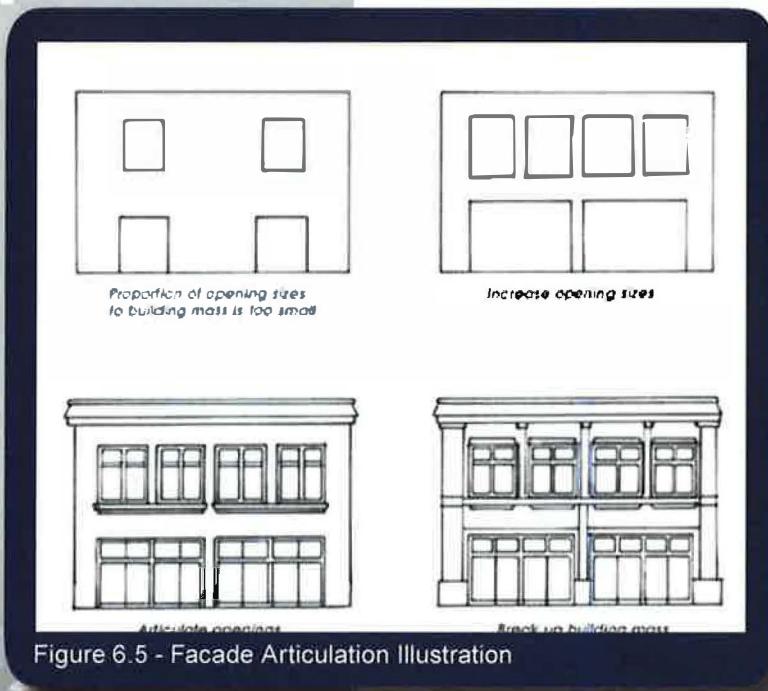


Figure 6.5 - Façade Articulation Illustration



Façade Articulation:

Street facing building facades, as well as all facades that front a plaza or pedestrian way should be articulated to improve the quality of the design.

6.D.13: Appropriate methods of articulation include, but are not limited to, the following (as illustrated in Figure 6.5):

1. Increasing the number and/or size of window openings;
2. Creating a defined base for the building;
3. Recessing storefronts, windows and/or entry ways to create depth and cast shadow patterns;
4. Providing three-dimensional expression lines (vertical and horizontal) between the floors of the structure and around storefronts and window openings; and
5. Adding depth and detail to the cornice or roof parapet.

Building Materials:

6.D.14: Appropriate building materials that are encouraged on facades include:

- Rock, stone, or tile
- Brick
- Plaster or stucco
- Concrete or masonry
- Finished and painted horizontal and vertical wood siding
- Fiber cement siding
- Wrought iron
- Finished and painted wood trim
- Wood, aluminum, copper, steel, and vinyl clad wood frames for windows and doors
- Wood, metal, and glass doors
- Other materials of similar quality, durability, and character to those listed above as approved by the approving body.

6.D.15: Inappropriate building materials that are discouraged on facades include:

- Plywood
- Hardboard
- Unfinished lumber
- Corrugated fiberglass
- Vinyl siding
- Sheet metal or tin siding other than for minor detail use



6.D.16: Painted building surfaces should have a matte or low-gloss finish. Trim work may have a glossy finish.



Parking

A. Introduction

While the City has taken great efforts to promote the use of non-automotive modes of transportation within the community, and certainly within the development of the LCO area, it is still necessary to make reasonable accommodations to provide vehicular parking to service the ultimate development of properties within the LCO area.

In order to provide for the greatest flexibility in serving the varying parking demands that are anticipated to occur with the desired mixed-use development within the LCO, the development and provision of public parking within the LCO is encouraged.

It is anticipated that the majority of this public parking (estimated as 350 potential parking stalls) will be provided via on-street public parking dispersed throughout the LCO area along Legacy Avenue and the Jackson Street Connection and frontage improvements along Sixth Street and College Street.

B. Off-Street Parking Requirements

7.B.1: Vehicular parking should be required for developments within the LCO area provided in accordance with the off-street parking requirements of the Urban Mixed Commercial (UMC) Zoning District, following standards:

Use of Building or Site	Minimum Number of Parking Spaces Required
Residential Uses	1 Stall per Dwelling Unit
Hotel	1 Stall per Room
Convention/Meeting Space	1 per 10 fixed seats and 1 per each 100 sq. ft. of assembly space without fixed seating

7



Use of Building or Site	Minimum Number of Parking Spaces Required
Office, Retail, and Other Uses	No minimum parking specified if provisions for the creation of public parking are provided through the dedication and improvement of public streets in accordance with this plan

C. Off-Street Parking Placement

7.C.1: Off-street parking facilities should not be located between structures and adjacent public right-of-ways or pedestrian corridors.

7.C.2: Off-street parking should be located underneath, behind, or to the side of all principle structures, or within parking structures.

7.C.3: Other than public parking facilities, surface parking lots should be minimized in order to reduce the detrimental impacts of large surface parking lots within the LCO area.

7.C.4: Where podium parking is provided it should be screened from public view by the building with habitable space or other building features.

D. Off-Street Parking Joint Use Provisions

Shared joint use of off-street parking facilities among multiple properties is encouraged and may be approved by the approving body.

7.D.1: Any proposal of shared joint use must include a detailed parking demand analysis including projected timing and volume of parking demand generated by the proposed uses and other relevant information deemed necessary to assess the joint-use request.

E. Off-Street Surface Parking Landscape Guidelines

Where off-street surface parking is provided, such parking areas should be adequately landscaped to visually buffer the vehicular parking area from adjacent streets and provide summer shading of the area.



7.E.1: Surface parking areas should include one canopy tree placed within or around the perimeter of the parking area for each five (5) stalls provided.

7.E.2: Off-street surface parking areas should be screened from adjacent public streets or pedestrian corridors by a five (5) foot wide landscape strip with shrubs or similar plantings that will provide adequate visual screening of the area at maturity and planted four (4) feet on center. The landscape strip may be reduced to three (3) feet in width with the inclusion of a fence or wall used for screening purposes.

F. Parking Structure Design Guidelines

While parking structures are a preferred method for the provision of off-street parking within the LCO area, care must be taken to ensure that the visual appearance and scale of such facilities are in keeping with the intended character of the LCO area. Any proposed parking structure within the LCO area should comply with the following:

7.F.1: The exterior design of parking structures should minimize its visual identity as parking through façade articulation, window openings and variations in color, material and/or texture.

7.F.2: Where possible, parking structures should be placed in a manner to minimize interruption of street frontage and screened from view by habitable space of the uses that they serve.

7.F.3: Where parking structures are located adjacent to public streets or pedestrian areas, the exterior of the parking structure should include architectural detail such as decorative façade features, planters, and storefronts to minimize the appearance of the parking structure to motorists and pedestrians.



Public Spaces and Amenities

8

A. Introduction

Integration of public spaces and similar amenities are vital in creating a sense of place and enhancing the social interaction and wellbeing of a neighborhood and community.

B. Key Public Spaces

8.B.1: Developments within the LCO area should incorporate public open spaces and features (such as plazas, fountains, seating and gathering areas and pocket parks) as key focal points within the LCO area as identified within Figure 8.1 and Appendix C.

C. Natural and Riparian Areas

The LCO area includes a portion of Paradise Creek and its associated riparian and floodplain areas. It is anticipated that the enhancement of these riparian areas will provide a recreational and visual amenities to the LCO area that should be integrated within adjacent developments.

8.C.1: Development proposals should provide for the protection of these sensitive areas and incorporate their natural beauty within the development to preserve these areas, minimize property damage from flooding and provide for passive recreational opportunities.

D. Public Art Integration and Key Installation Locations

Public art and cultural amenities build artistic and creative energy



within the community and enhance economic and social vitality. Public art installations create a sense of place and promote cultural tourism as an economic industry.



Figure 8.2 - Public Art Example

8.D.1: Public art incorporation within building facades, street furnishings, public installations and other development components are highly encouraged.

8.D.2: Developments should incorporate public art installations within all proposed key public spaces and the identified installation locations identified in Figure 8.1 and Appendix C.



Sustainable Development Practices

9

A. Stormwater Management

9.A.1: To the greatest extent possible all developments within the LCO area should utilize best management practices to capture, detain, reuse, treat and otherwise control and reduce stormwater runoff from the development site. Such management plans should include items such as pervious pavement materials, bioswales, green roofs, rainwater harvesting, and other such practices.

9.A.2: All developments within the LCO area should integrate low impact stormwater management practices in addition to meeting all other stormwater requirements and provisions of Moscow City Code.

B. Public Transit Facilities

Developments within the LCO area should provide for the logical location and distribution of public transit facilities to promote greater utilization of public transit services in the area and reduce automobile parking demand.

9.B.1: All developments within the LCO area should assess the need for transit facilities necessary to serve the development including the location of public transit stops, benches, shelters and signage.

C. Bicycle Parking Facilities

Developments within the LCO area should provide for the logical location and distribution of bicycle parking facilities to promote greater utilization of bicycles as a mode of transportation to and from the area and reduce automobile parking demand within the area.

9.C.1: Developments within the LCO area should provide bicycle parking for each use of a lot or structure in accordance with the following standards:



Section Nine

Use of Building or Site	Minimum Number of Parking Spaces Required
Residential Uses	1 Bicycle Space per Each Two (2) Dwelling Units
Hotel	1 Bicycle Space per Each Ten (10) Rooms
Convention/Meeting Space	1 Bicycle Space for Each Ten (10) Automobile Parking Spaces Provided
Office, Retail, and Similar Uses	1 Bicycle Space for Each 1,000 sq. ft.

9.C.2: Each bicycle parking space shall be sufficient to accommodate a bicycle at least six (6) feet in length and two feet wide, and shall be provided with some form of stable frame permanently anchored to a foundation to which a bicycle frame and both wheels may be conveniently secured using a chain and padlock, locker, or other storage facilities which are convenient for storage and are reasonably secure from theft and vandalism.

9.C.3: The separation of the bicycle parking spaces and the amount of corridor space must be adequate for convenient access to every space when the parking facility is full.

9.C.4: When automobile parking spaces are provided in a structure, all required bicycle spaces shall be located inside that structure or should be located in other areas protected from the weather. Bicycle parking spaces in parking structures should be clearly marked as such and should be separated from auto parking by some form of barrier to minimize the possibility of damage from a vehicle.

