



Meeting Agenda: Thursday, November 7, 2024, 7:30 a.m.

City of Moscow Council Chambers • 206 E 3<sup>rd</sup> Street • Moscow, ID 83843  
(A) = Board Action Item

1. **Consent Agenda (A)** - Any item will be removed from the consent agenda at the request of a member of the Board and that item will be considered separately later.
  - A. Minutes from October 17, 2024
  - B. September 2024 Payables
  - C. September 2024 Financials**ACTION:** Approve the consent agenda or take such other action deemed appropriate.
2. **Public Comment**

Members of the public may speak to the Board regarding matters NOT on the Agenda nor currently pending before the Moscow Urban Renewal Agency. Please state your name and resident city for the record and limit your remarks to three minutes.
3. **Discussion of Next Steps for the Agency's Sixth and Jackson Street Property (A) – Cody Riddle**

Staff will lead a discussion regarding potential improvements and interim use of the Agency's property at Sixth and Jackson.

**ACTION:** Discuss and provide Staff with direction as deemed appropriate.
4. **2025 Strategic Plan Update (A) – Cody Riddle**

In 2020, the Board adopted the current, five-year Strategic Plan that includes goals and objectives to guide the activities of the Agency. The plan will expire in the coming year and the community has evolved since its adoption. Staff will lead a discussion focused on the background, goals, objectives and strategies for an update to the plan.

**ACTION:** Discuss the goals, objectives and strategies for the Agency over the next five to seven years, and provide staff with direction on draft language for an update to the plan.
5. **General Agency Updates – Cody Riddle**
  - General agency business

**NOTICE:** It is the policy of the City of Moscow that all City-sponsored public meetings and events are accessible to all people. If you need assistance in participating in this meeting or event due to a disability under the ADA, please contact the City's ADA Coordinator by phone at (208) 883-7600, TDD (208) 883-7019, or by email at [adacoordinator@ci.moscow.id.us](mailto:adacoordinator@ci.moscow.id.us) at least 48 hours prior to the scheduled meeting or event to request an accommodation. The City of Moscow is committed to ensuring that all reasonable accommodation requests are fulfilled.



Meeting Minutes: October 17, 2024, 7:30 a.m.

City of Moscow Council Chambers • 206 E 3<sup>rd</sup> Street • Moscow, ID 83843

Commissioners Present	Commissioners Absent	Staff in Attendance
Steve McGeehan, Chair	Mark Beauchamp	Cody Riddle, Executive Director
Drew Davis	Sandra Kelly	Jennifer Fleischman, Clerk
Tom Lamar	Nancy Tribble	Renee Tack, Treasurer
Alison Tompkins		

McGeehan called the meeting to order at 7:33 a.m.

**1. Consent Agenda (A)**

*Any item will be removed from the consent agenda at the request of any member of the Board and that item will be considered separately later.*

- A. Minutes from August 15, 2024
- B. August 2024 Payables
- C. August 2024 Financials

Riddle explained what the Owner Participation Agreement (OPA) expenses are.

Lamar moved for approval of the consent agenda as written, seconded by Davis. Vote by Acclamation: Ayes: Unanimous (4). Nays: None. Abstentions: None. Motion carried.

**2. Public Comment**

*Members of the public may speak to the Board regarding matters NOT on the Agenda nor currently pending before the Moscow Urban Renewal Agency. Please state your name and resident city for the record and limit your remarks to three minutes.*

None.

**3. University of Idaho Long-Range Planning Update - Céline Acord**

*University Staff will provide an update on campus master planning efforts, including those in the vicinity of the Agency's Legacy Crossing property.*

Discussed as Agenda Item No. 4.

The Chair introduced Celine Acord and Ray Pankopf from the University of Idaho (UI). Acord described some of the different ideas that UI has in moving forward with their campus planning. The design concepts are currently focused on the core of campus and then UI will move to planning some of the outer edges of campus property. There was a brief review of the improvements on campus over the last 25 years. Some other aspects of the master plan updates include Athletic facilities, Recreation/Wellness and Student Health, and a new School of Health and Medical Professionals.

UI staff reviewed several different planning options that focus on different important aspects of campus. All of the scenarios include a significant addition to the Kibbie Dome, for either an elevated fan experience or more room for Athletics facilities. A large focus was on solidifying the Hello Walk connection with downtown Moscow.

The proposed construction date for the Hello Walk bridge would be in 2026. The bridge is completely funded but has not been designed yet.

**4. Professional Services Agreement with Presnell Gage for Audit Services (A) – Cody Riddle**

*The Agency has utilized the services of Presnell Gage for the annual audit for a number of years. Staff is seeking board approval to execute a five (5) year professional services agreement for completion of the annual audit. The cost of the proposed agreement will start at \$6,050 for 2024 and increase incrementally to \$6,650, for the last year of the agreement in 2028.*

Discussed as Agenda Item No. 3.

Riddle provided the Board a short history of the Agency’s relationship with Presnell Gage and explained the agreement for their services for the next five (5) years, as described above. The Board talked about the great work Presnell Gage has been doing over the years.

Davis moved to approve the Professional Services Agreement for the next five (5) years of Agency audits with Presnell Gage, seconded by Lamar. Vote by Roll Call: Ayes: Unanimous (4). Nays: None. Abstentions: None. Motion carried.

**McGeehan left at 7:58 PM and the meeting lost quorum.**

\_\_\_\_\_  
Steve McGeehan, Agency Chair

\_\_\_\_\_  
Date



**Balance Sheet**  
**September 30, 2024**

	Total Funds
<b>ASSETS</b>	
Cash	26,216
Investments - LGIP	3,618,256
Investments-Zions Debt Reserve	44,524
Other Assets	5,260
Taxes Receivable	47,089
Interest Receivable	15,345
Land	679,420
<b>Total Assets</b>	<b>\$ 4,436,111</b>
 <b>LIABILITIES</b>	
Accounts Payable	373
Series 2010 Bond - due within one year	39,000
Latah County payback agreement - due within one year	5,000
Series 2010 Bond - due after one year	82,000
Latah County payback agreement - due after one year	69,537
Total Liabilities	195,910
 <b>FUND BALANCES</b>	
Net Investment in Capital Assets	558,420
Restricted Fund Balance	44,312
Unrestricted Fund Balance	3,637,469
Total Fund Balance	4,240,201
 <b>Total Liabilities and Fund Balance</b>	 <b>\$ 4,436,111</b>



September-24  
Checks by Date



Check Number	Vendor	Description	Check Date	Check Amount
4954	UAVISTA 1563734669-09162024	Avista Utilities Aug'24 Electric for Legacy Property	09/05/2024	43.27
Total for Check Number 4954:				<u>43.27</u>
4955	UCITYMOS 115911-08312024	City of Moscow Aug'24 Utilities 6th & Jackson	09/05/2024	331.47
Total for Check Number 4955:				<u>331.47</u>
4956	UCITYMOS 2400002327	City of Moscow City Admin Fees Sept'24	09/12/2024	4,750.42
Total for Check Number 4956:				<u>4,750.42</u>
4957	UCITYMOS PRFY24-1	City of Moscow 6th & Jackson Snow Removal	09/26/2024	414.63
Total for Check Number 4957:				<u>414.63</u>
<b>Total bills for September 2024:</b>				<b><u><u>\$ 5,539.79</u></u></b>

September-24  
 Accounts Payable Checks for Approval



Check	Check Date	Fund Name	Vendor	Void	Amount
4954	09/05/2024	Moscow Urban Renewal Agency	Avista Utilities		43.27
4955	09/05/2024	Moscow Urban Renewal Agency	City of Moscow		331.47
4956	09/12/2024	Moscow Urban Renewal Agency	City of Moscow		4,750.42
4957	09/26/2024	Moscow Urban Renewal Agency	City of Moscow		414.63
Report Total:				<u>0.00</u>	<u>5,539.79</u>

\_\_\_\_\_  
 Steve McGeehan, Chairperson

\_\_\_\_\_  
 Cody Riddle, Executive Director

Accounts payable expenditures as contained herein were made in compliance with the duly adopted budget for the current fiscal year and according to Idaho law.

\_\_\_\_\_  
 Renee Tack, Treasurer

General Ledger  
Expense vs. Budget

September-24



<b>Account</b>	<b>Description</b>	<b>Amended Budget</b>	<b>Period Amt</b>	<b>End Bal</b>	<b>Variance</b>	<b>% Budget Used</b>
	URA General Fund					
890-880-642-00	Administrative Services	\$ 57,005.00	\$ 4,750.42	\$ 57,005.04	\$ (0.04)	100.00%
890-880-642-15	Professional Services-Other	\$ 5,000.00	\$ -	\$ 1,275.00	\$ 3,725.00	25.50%
890-880-642-20	Professional Services-Auditing	\$ 5,871.00	\$ -	\$ 5,950.00	\$ (79.00)	101.35%
890-880-642-89	Professional Services	\$ 525.00	\$ -	\$ 669.95	\$ (144.95)	127.61%
890-880-644-10	Advertising & Publishing	\$ 500.00	\$ -	\$ 249.82	\$ 250.18	49.96%
890-880-668-10	Liability Insurance-General	\$ 1,950.00	\$ -	\$ 2,172.00	\$ (222.00)	111.38%
	Contractual	\$ 70,851.00	\$ 4,750.42	\$ 67,321.81	\$ 3,529.19	95.02%
890-880-631-10	Postage Expense	\$ 100.00	\$ -	\$ -	\$ 100.00	0.00%
890-880-631-20	Printing and Binding	\$ 400.00	\$ -	\$ -	\$ 400.00	0.00%
890-880-647-10	Travel & Meetings-General	\$ 500.00	\$ -	\$ -	\$ 500.00	0.00%
890-880-649-10	Professional Development	\$ 500.00	\$ -	\$ -	\$ 500.00	0.00%
890-880-669-10	Misc. Expense-General	\$ 500.00	\$ -	\$ 22.50	\$ 477.50	4.50%
	Commodities	\$ 2,000.00	\$ -	\$ 22.50	\$ 1,977.50	1.13%

General Ledger  
Expense vs. Budget

September-24



		<b>Amended</b>									
	URA General Fund - Total	\$	72,851.00	\$	4,750.42	\$	67,344.31	\$	5,506.69	92.44%	
URA Legacy District											
890-895-642-10	Professional Services-Legacy	\$	5,150.00	\$	-	\$	-	\$	5,150.00	0.00%	
890-895-642-12	Land Sale Expense-Legacy	\$	2,060.00	\$	-	\$	-	\$	2,060.00	0.00%	
890-895-644-10	Ad. & Marketing Expense-Legacy	\$	1,030.00	\$	-	\$	-	\$	1,030.00	0.00%	
	Contractual	\$	8,240.00	\$	-	\$	-	\$	8,240.00	0.00%	
890-895-647-10	Travel & Meetings-Legacy	\$	515.00	\$	-	\$	-	\$	515.00	0.00%	
890-895-652-10	Heat, Lights & Utilities	\$	4,635.00	\$	747.39	\$	4,627.10	\$	7.90	99.83%	
890-895-658-51	Development Participation	\$	870,000.00	\$	-	\$	-	\$	870,000.00	0.00%	
890-895-669-10	Misc. Expense-Legacy	\$	515.00	\$	414.63	\$	414.63	\$	100.37	80.51%	
890-895-675-00	Fiscal Agent Trustee fees	\$	1,500.00	\$	-	\$	1,500.00	\$	-	100.00%	
890-895-676-15	Latah County Reimb. Agreement	\$	5,000.00	\$	-	\$	5,000.00	\$	-	100.00%	

General Ledger  
Expense vs. Budget

September-24



		<b>Amended</b>					
890-895-676-17	Owner Participation Agreements	\$ 63,490.00	\$ -	\$ 43,825.74	\$ 19,664.26	69.03%	
	Commodities	\$ 945,655.00	\$ 1,162.02	\$ 55,367.47	\$ 890,287.53	5.85%	
890-895-890-00	Transfer To: General Fund	\$ 72,851.00	\$ 67,344.31	\$ 67,344.31	\$ 5,506.69	92.44%	
	Transfers To	\$ 72,851.00	\$ 67,344.31	\$ 67,344.31	\$ 5,506.69	92.44%	
890-895-900-11	Contingency - Legacy	\$ 15,000.00	\$ -	\$ -	\$ 15,000.00	0.00%	
	Contingency	\$ 15,000.00	\$ -	\$ -	\$ 15,000.00	0.00%	

General Ledger  
Expense vs. Budget

September-24



		<b>Amended</b>								
	URA Legacy District - Total	\$	1,041,746.00	\$	68,506.33	\$	122,711.78	\$	919,034.22	11.78%
890-892-790-01	Bond Principal - Legacy	\$	37,000.00	\$	-	\$	37,000.00	\$	-	100.00%
890-892-791-01	Bond Interest - Legacy	\$	6,936.00	\$	-	\$	1,987.58	\$	4,948.42	28.66%
	Debt Service - Total	\$	43,936.00	\$	-	\$	38,987.58	\$	4,948.42	88.74%
890-892-900-01	Ending Fund Bal - Assigned	\$	999,103.00	\$	-	\$	-	\$	999,103.00	0.00%
890-892-990-05	Ending Fund Bal - Restricted	\$	49,752.00	\$	-	\$	-	\$	49,752.00	0.00%
890-899-990-00	Ending Fund Bal - Unassigned	\$	190,391.00	\$	-	\$	-	\$	190,391.00	0.00%
	Ending Fund Balance - Total	\$	1,239,246.00	\$	-	\$	-	\$	1,239,246.00	0.00%
TOTAL	Moscow Urban Renewal Agency	\$	2,397,779.00	\$	73,256.75	\$	229,043.67	\$	2,168,735.33	9.55%

General Ledger  
Revenue Analysis

September 2024



Account Number	Description	Budgeted Revenue	Period Revenue	YTD Revenue	Variance	Uncollected Bal	% Avail/Uncollect	% Received
<b>890</b>	<b>Moscow Urban Renewal Agency</b>							
890-000-410-01	Property Taxes - Legacy	\$ 988,278.00	\$ 56,742.33	\$ 1,019,437.63	\$ (31,159.63)	\$ (31,159.63)	-3.15%	103.15%
890-000-471-00	Investment Earnings	\$ 45,000.00	\$ 31,374.63	\$ 168,903.35	\$ (123,903.35)	\$ (123,903.35)	-275.34%	375.34%
890-000-498-96	Transfer In: Legacy	\$ 72,851.00	\$ 67,344.31	\$ 67,344.31	\$ 5,506.69	\$ 5,506.69	7.56%	92.44%
<b>890</b>	<b>Moscow Urban Renewal Agency</b>	<b>\$ 1,106,129.00</b>	<b>\$ 155,461.27</b>	<b>\$ 1,255,685.29</b>	<b>\$ (149,556.29)</b>	<b>\$ (149,556.29)</b>	<b>-13.52%</b>	<b>113.52%</b>
<b>Revenue Total</b>		<b>\$ 1,106,129.00</b>	<b>\$ 155,461.27</b>	<b>\$ 1,255,685.29</b>	<b>\$ (149,556.29)</b>	<b>\$ (149,556.29)</b>	<b>-13.52%</b>	<b>113.52%</b>



# Legacy Crossing Overlay District Design Guidelines

Adopted June 4, 2012







# Contents

Introduction.....	1
External Site Ingress and Egress.....	5
Internal Site Access and Circulation.....	7
Streetscape Design Guidelines.....	11
Pedestrian Connectivity.....	15
Building Design Guidelines.....	17
Parking.....	23
Public Spaces and Amenities.....	27
Sustainable Development Practices.....	29



1

# Introduction

## A. Community Vision

Throughout the fall of 2007 and spring of 2008, University of Idaho architecture students, property and business owners, residents, and elected officials engaged in a series of collaborative charettes and public meetings to develop a community-based vision for the future of the Legacy Crossing Urban Renewal District.

The vision captured the community's desire to bridge the divide between the University of Idaho Campus and the Downtown and energize and revitalize the core of the community. Throughout the process it was clear that the community envisioned the development of this area with high intensity dynamic and vibrant mixed uses.

Example illustrations of this vision are shown in Figure 1.1. These design guidelines were developed to facilitate the implementation of the vision.



## B. Subject Area

The Legacy Crossing Overlay District (LCO) is that area (shown in Figure 1.2, and illustrated in more detail in Appendix A) generally bounded by Sixth Street to the north, Sweet Avenue to the west, Jackson Street to the east, and State Highway 95 to the south. This



area is generally located between the University of Idaho campus and the central business district and which has been historically utilized for agricultural, industrial and rail transportation uses.



Figure 1.2 - Subject Area

### C. Purpose and Intent

In consideration of the unique characteristics of the LCO area, the redevelopment of this area requires an increased level of attention and consideration of site ingress and egress; building architectural style, placement and massing; public streetscape design and function; internal vehicular and pedestrian circulation; parking provision and management; preservation and enhancement of riparian areas; and the integration of public community spaces, public art and other public enhancements.

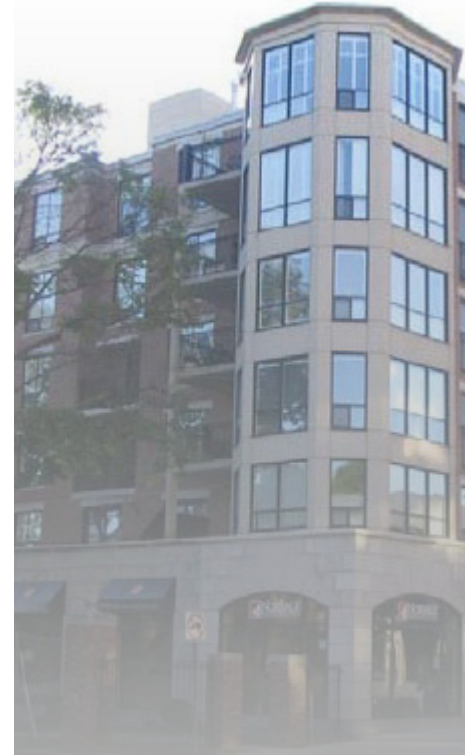
These development guidelines are intended to complement the City's Zoning Code by establishing development standards and guidelines for developments within the LCO. These guidelines are to be utilized by City Staff, the Planning and Zoning Commission, and City Council in their review of all developments within the LCO area.

The items included within this document are intended to be guidelines, and as such, minor deviations or variations may be allowed by the reviewing body where justified by the specific limitations or constraints of the development site, project layout and where such variations and deviations do not conflict with the intents and purposes of these Design Guidelines.



## D. Land Uses

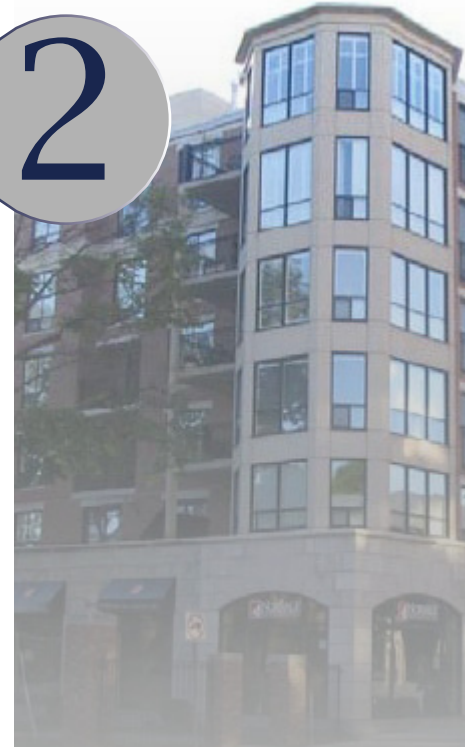
The redevelopment of the LCO area is generally anticipated to include a wide range of mixed-uses including retail, artists studios, eating and drinking establishments, limited light manufacturing where appropriate, professional office space as well as residential dwelling units. These design guidelines are generally intended to address the physical development of the LCO area rather than specific uses which are to be guided by the underlying zoning of the LCO area.





# External Site Ingress & Egress

## 2



### A. Introduction

This section addresses the exterior boundaries of the site and their relationship with the adjacent city street system as it pertains to logical and desirable ingress and egress points upon adjacent streets. As the site is principally bounded by a national highway (U.S. HWY 95) and a collector street that serves as one of the principal transportation corridors between the University of Idaho and the downtown and residential areas to the east (Sixth Street), planning for the safe and efficient ingress and egress to and from the site is critical to the successful redevelopment of the area and the safe function of the adjacent streets.

### B. Surrounding City Streets

The site is generally bounded by Sixth Street to the north, Jackson (U.S. Highway 95) to the east, and Sweet Avenue to the south. The site is also bisected by College Street through the center of the LCO area.

Sixth Street adjacent to the LCO area is a designated collector street with widened urban sidewalks near newer developments and structures primarily located at or very near the property lines.

Jackson Street adjacent to the subject property is a three-lane street section that serves as the southbound segment of U.S. Highway 95 through the City. Sidewalks along Jackson are present in limited locations and are of insufficient width to meet an urban walk standard. Uses adjacent to Jackson are primarily the few remaining agricultural/industrial uses and structures, with the exception of the area near the Sixth Street intersection.

College Street bisects the LCO area and in many areas lacks any curbing, sidewalks and on-street parking facilities. Adjacent uses and structures are primarily agricultural/industrial in nature or undeveloped lands.

## Section Two



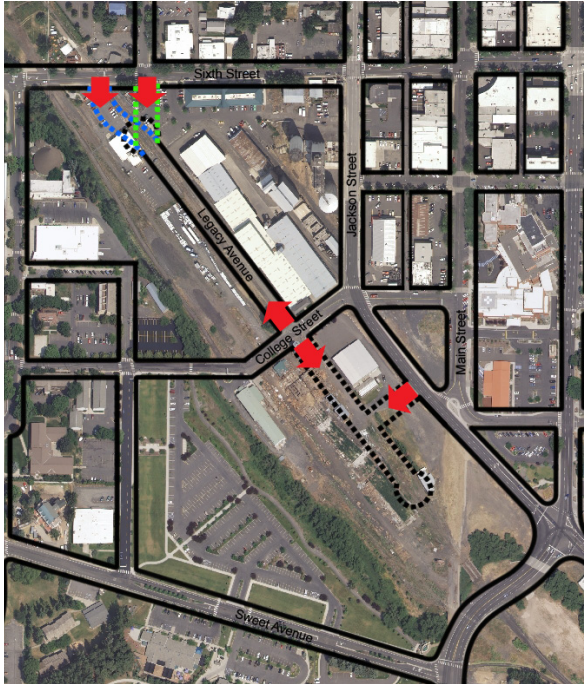


Figure 2.1 - External Site Access Points

## C. Site Access Locations

Principal vehicular access points to the site should be located so as to permit smooth traffic flow in and out of the site that minimize hazards to vehicular or pedestrian traffic upon adjacent public streets, pathways and sidewalks. As such, developments within the LCO area should incorporate the following features:

**2.C.1:** Principle access points to the development site should be located opposite existing street alignments and intersections.

**2.C.2:** The number of access points upon adjacent streets should be limited to minimize hazards and promote logical and orderly shared access and internal circulation within the LCO.

**2.C.3:** The preferred principal access locations are illustrated in Figure 2.1.



# Internal Site Access and Circulation

## 3

### A. Introduction

To facilitate the orderly development of the LCO it is necessary to ensure that there is adequate internal circulation and access to allow for the full development of the site. In consideration of the configuration of property ownership and the physical dimensions of the site, it is clear that internal circulation and access would be enhanced with the extension of a street through the center of the site as well as a secondary access point via Jackson Street as illustrated in Figure 4 and Appendix A.

These public streets will provide for the creation of significant commercial frontage that currently does not exist as well as provide access to customers, emergency service providers, public utilities, service and delivery, as well as the provision of public on-street parking to serve adjacent land uses.

### B. Legacy Avenue

The existing property boundary configuration which reflects the historical railroad corridors does not facilitate the logical and orderly development of the LCO area, nor does it provide for efficient circulation and service delivery. Many properties within the LCO area are long and narrow, lack street frontage and access, and have no block structure to build upon. To address this condition, it is proposed that a public street be developed through the center of the LCO area on an alignment that roughly parallels Paradise Creek. In addition to providing the principal access to the site and the adjacent developable properties, this public street corridor will create significant commercial frontage and the opportunity to provide public parking and other amenities to serve development within the area.

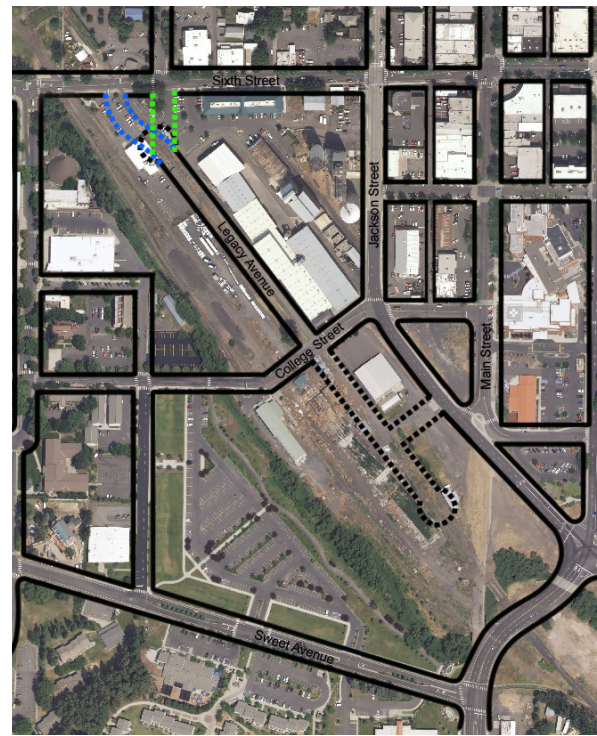


Figure 3.1 - Internal Site Access and Circulation





Primary access to this new roadway, referred hereinafter as Legacy Avenue, would be gained from College Street and extend northwesterly and southeasterly from its intersection with College Street. The City is in the process of securing the majority of the required right-of-way for the extension of Legacy Avenue north of College Street. It is envisioned that the northwesterly extension of Legacy Avenue may terminate temporarily, or permanently, in a traffic circle south of current Taco Time Restaurant, or it may continue northwesterly to an intersection of Sixth Street in some configuration as development or redevelopment occurs within the area.

The southeasterly extension of Legacy Avenue is envisioned to extend from College Street and then turn northeasterly to connect to Jackson Street. This connection from College Street to Jackson Street would provide multiple points of access and enhanced circulation for vehicle access and emergency service provision to the three larger parcels within the area south of College Street. It is possible that access and cross circulation could be provided in the form of private roadways and accompanying public access easements that provide the equivalent functional access, circulation, and development form, depending upon the proposed development and subject to the approval of the reviewing body.

It is anticipated that public right-of-way necessary to accommodate Legacy Avenue would range from 70 to 85 feet in width depending upon on-street parking configuration as illustrated in the proposed street sections within Section 4.

**3.B.1:** All developments within the LCO should incorporate the extension of Legacy Avenue in conformance with the approved street sections described in these guidelines.

## C. Jackson Street Connection

While Legacy Avenue provides for the internal access and circulation, the Jackson Street Connection into the site provides for a consolidated shared access to Jackson Street which serves as the southbound half of the U.S. HWY 95 couplet that encompasses the downtown area.

While the LCO area has a significant length of frontage along Jackson Street, due to its function as U.S. Highway 95 and the Idaho Transportation Department's access management program, access to the LCO site via Jackson Street is limited to ensure that entrance and exit turning movements do not jeopardize the safety and traffic carrying capacity of the adjacent highway. Therefore, the management and provision of a consolidated shared access to Jackson Street via





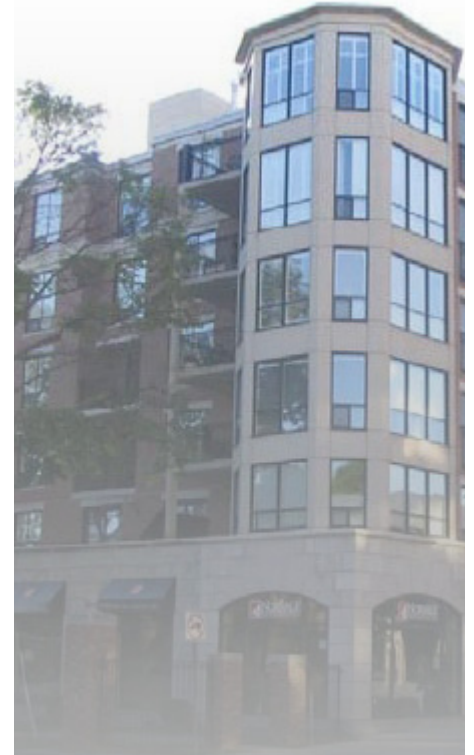
the Legacy/Jackson Connector is necessary.

**3.C.1:** All developments within the LCO should incorporate the Jackson Street Connection to intersect with Legacy Avenue to serve as a consolidated point of access to Jackson/U.S. Highway 95.

## D. Loading Zones, Service Areas, and Refuse Management

**3.D.1:** Where feasible, rear service and loading areas should be provided, via public alley or private drive access, to direct such activities away from the public streets and commercial frontages.

**3.D.2:** Dumpsters and similar trash and recycling receptacles shall be located and physically screened from view from adjacent public streets and walkways.



# Streetscape Design Guidelines

## 4

### A. Introduction

Streets and sidewalks are key to the livability, vitality and identity of urban areas. Well designed streets and sidewalks add value and act as a catalyst to the development of private property. They provide the setting for land use, and the capacity of the street and sidewalk systems to carry vehicular and pedestrian traffic determines, in part, the appropriate adjacent land uses. Sidewalk width, street lights and other amenities affect pedestrian activity and aesthetic quality and thus help to determine how adjoining private land may be developed and used.

### B. Urban Street and Sidewalk Standard

Urban sidewalks are generally located in business and mixed-use districts where there is a concentration of higher intensity development, ground floor retail, restaurants and entertainment uses, and on-street parking. They are designed for heavy pedestrian use and to be compatible with uses in a dense urban environment.

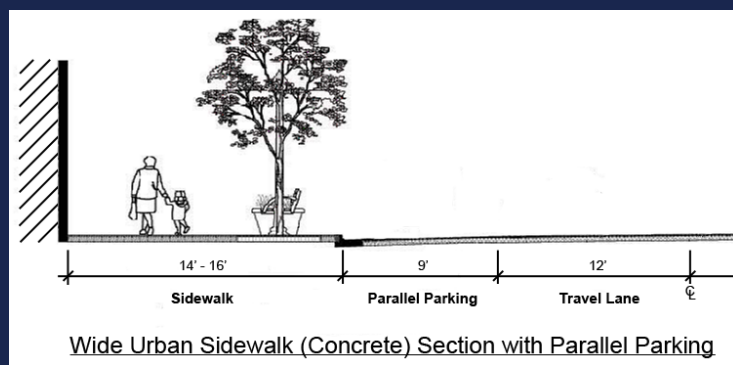
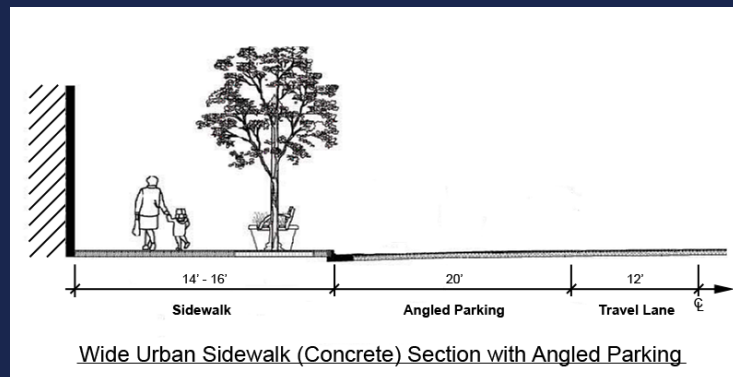
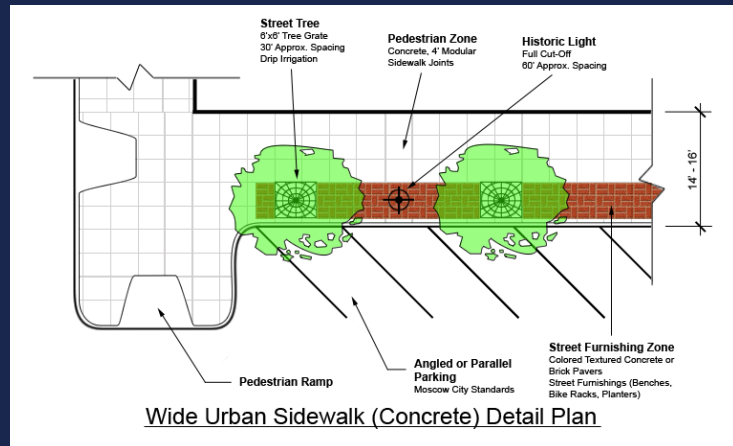


Figure 4.1 - Urban Sidewalk Standard Details

## C. Street Design Guidelines

### General Standards:

**4.B.1:** All public street frontages within the LCO area should be developed to a wide urban sidewalk including the provision of a minimum five (5') foot wide furnishing zone and minimum nine (9') foot wide pedestrian zone as illustrated in Figure 4.1 and Appendix A.

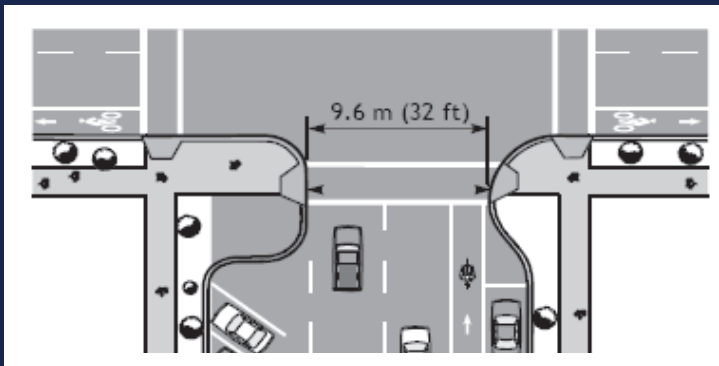


Figure 4.2 - Example Pedestrian Curb Extensions

**4.B.2:** The furnishing zone should include a minimum of five (5') foot diameter street tree wells with drip irrigation spaced approximately thirty (30) feet on center and full cut-off street light fixtures approximately sixty (60) feet on center, or as otherwise specified.

**4.B.3:** All public streets within the LCO should include on-street vehicular parking when possible via angled or parallel parking stalls.

**4.B.4:** All intersections and pedestrian crossings should include accessible pedestrian curb extensions (bump-outs) to promote pedestrian use and improve pedestrian safety as illustrated in Figure 4.2.

### Legacy Avenue:

Legacy Avenue is anticipated to serve as the primary pedestrian and vehicular internal access and circulation corridor through the LCO area, but will also serve as the primary public parking area to serve development in the LCO area.

Public parking provides for the greatest flexibility in parking utilization and if dispersed throughout the LCO area along the Legacy Avenue alignment, will provide conveniently located parking throughout the LCO area that will reduce the amount of parking that must be privately provided, thus increasing the developable area within the LCO area.

**4.B.5:** Developments adjacent to Legacy Avenue should include street improvements in accordance with Figure 4.1.



### Sixth Street:

Sixth Street borders the LCO site to the north. Recent development along Sixth Street (University Pointe) has included the development of a wider urban sidewalk and on-street parallel parking.

**4.B.6:** Developments adjacent to Sixth Street should include street improvements in accordance with Figure 4.1.

### Jackson Street Connection:

The Jackson Street Connection is anticipated to serve a similar function and be developed to a similar standard as Legacy Avenue.

**4.B.7:** All development within the LCO area should include the construction of the Jackson Street Connection in accordance with the proposed street design which includes a 12 to 16 foot wide urban sidewalk area, on-street parking areas as well as the vehicular travel lanes as shown in proposed street cross-section of Figure 4.1 and Appendix A.

### College Street:

College Street bisects the LCO site and should be developed in a similar fashion as that of the Legacy Avenue and Jackson Street Connection with angled parking bays and wider urban sidewalks as shown in the proposed street cross-sections.

**4.B.8:** Development adjacent to College Street should include street improvements in accordance with Figure 4.1.

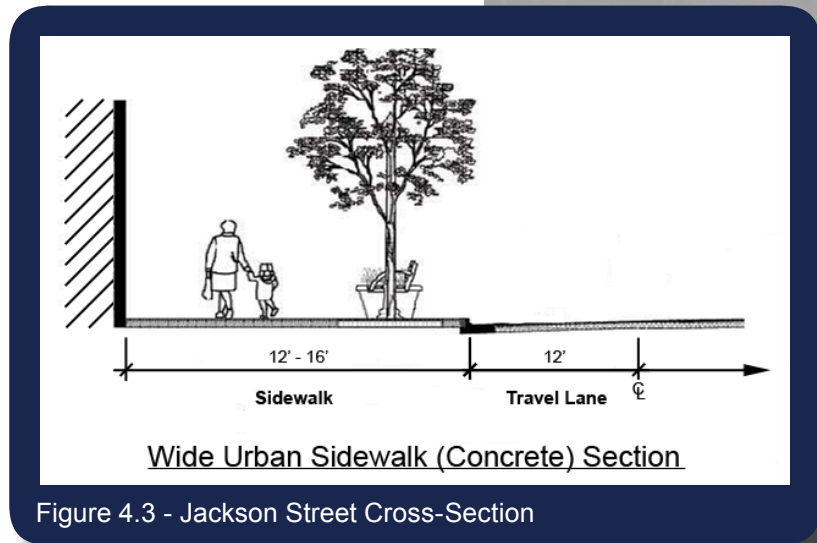
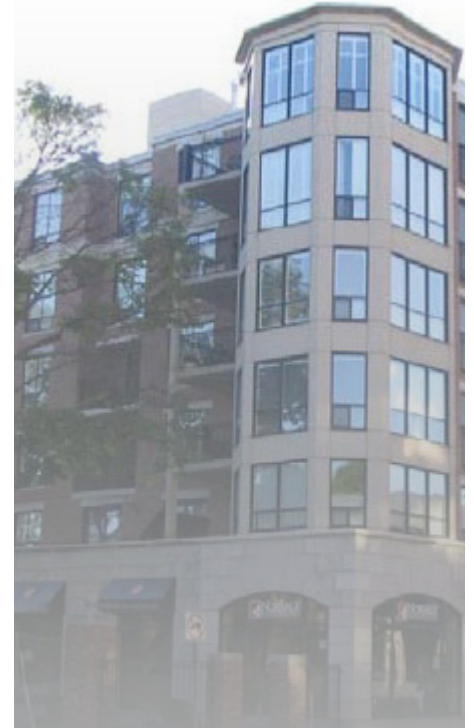


Figure 4.3 - Jackson Street Cross-Section

### Jackson Street:

Jackson Street adjacent to the LCO area is a three-lane, one-way street which currently serves as the southbound half of the U.S. HWY 95 corridor through the downtown area. Ultimately it is anticipated that at some future date U.S. 95 may be rerouted outside of the downtown area which will allow for the future reconfiguration of the street. In the interim, it is anticipated that the Jackson Street frontage will primarily include the installation of a street furnishing/tree zone as



well as a wider urban sidewalk as shown upon the proposed street cross-section shown in Figure 4.3.

**4.B.9:** Developments adjacent to Jackson Street should include street improvements in accordance with Figure 4.3.





# Pedestrian Connectivity

## 5

### A. Introduction

Due to the Redevelopment District's unique location between the University of Idaho Campus and the Central Business District, pedestrian access to and through the Redevelopment District is an important consideration. Developments proposed within the LCO should provide pedestrian facilities in accordance with the provisions of this Section.

### B. Principal Pedestrian Corridors

**5.B.1:** All development proposals should incorporate and provide for the three pedestrian ways illustrated in Figure 5.1 and Appendix B including:

1. The continuation of the "Hello-Walk" which extends from the southwest corner of the intersection of Sixth and Jackson Streets southwest to a location near the rear of the University of Idaho Bookstore facility in accordance with Figure 5.2;
2. The extension of the current Sweet Avenue parking lot central walkway in accordance with Figure 5.3; and
3. The historic Main Street alignment and bridge structure across the southeast corner of the LCO Area in accordance with Figure 5.3.

### C. Pedestrian Corridor Facilities

All pedestrian corridor facilities should be developed with the following design features and elements specified below:

**5.C.1:** All pedestrian corridor facilities should be surfaced with permanent hard surface of concrete unit pavers, concrete with

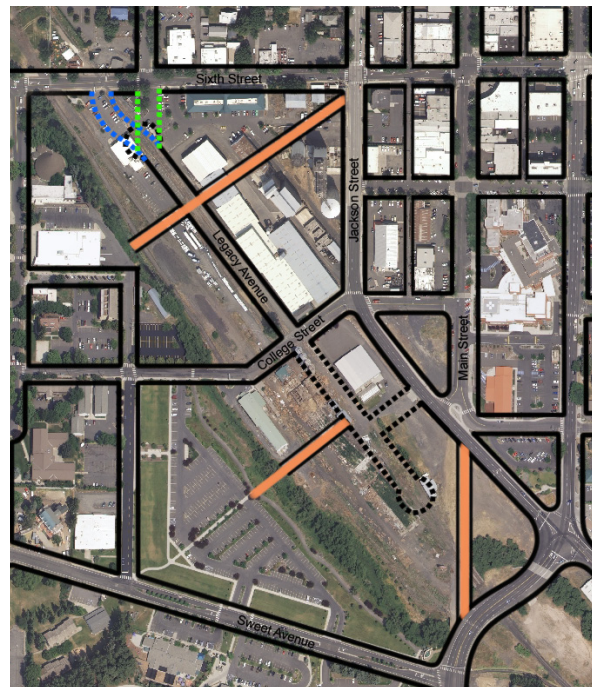


Figure 5.1 - Critical Pedestrian Corridors



special texture colors and patterns, brick or other paver blocks, or other similar materials as illustrated in Figure 5.2 and Figure 5.3.

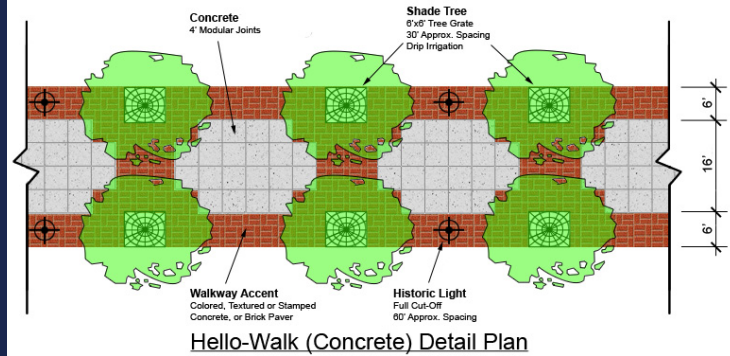


Figure 5.2 - Hello-Walk Corridor Detail

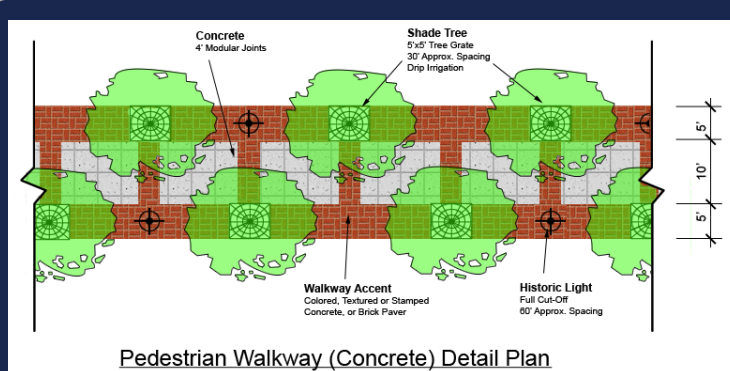


Figure 5.3 - Pedestrian Corridor Detail

**5.C.2:** All pedestrian corridor facilities should incorporate canopy trees and other vegetative plantings with drip irrigation to provide shade and shelter for pedestrians as illustrated in Figure 5.2 and Appendix B.

**5.C.3:** All pedestrian corridor facilities should include the integration of street furnishings (trash receptacles, benches, bollards, street lighting fixtures, bike racks, and other similar furnishings) as illustrated in Figure 5.2 and Appendix B.

**5.C.4:** All pedestrian corridor facilities should incorporate public art through surfacing patterns or designs, sculpture installation, and other means.

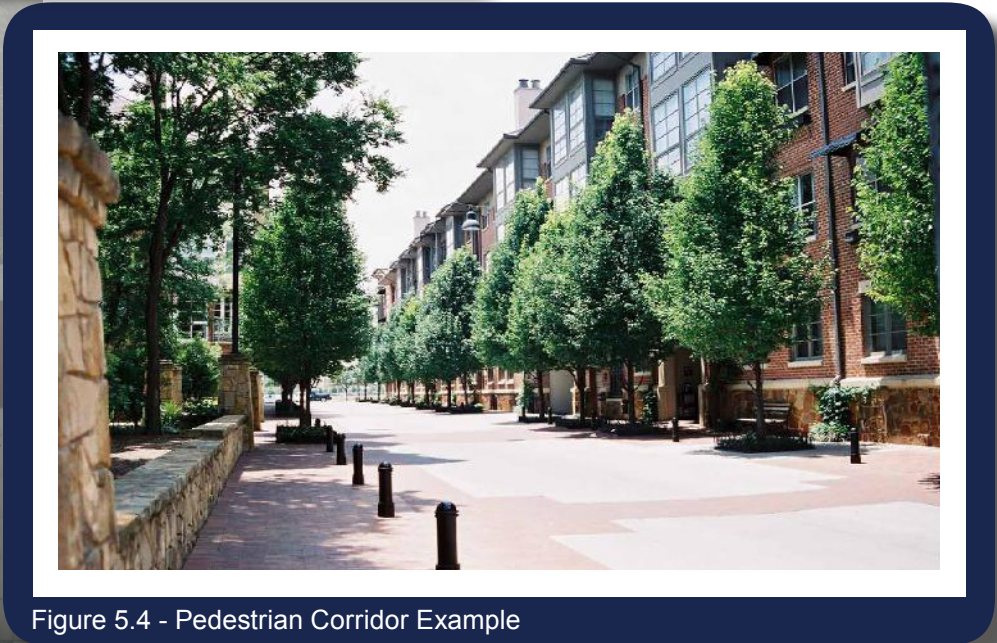


Figure 5.4 - Pedestrian Corridor Example





# Building Design Guidelines

## 6

### A. Introduction

A significant component to the ultimate realization of the community's vision for the development of the LCO area as a vibrant mixed-use pedestrian oriented urban center is the placement and character of the buildings that are constructed within the area. This includes building placement, height and architectural features (building materials and design) that together, with the other elements addressed in these guidelines, will assist in achieving the desired character of development in the LCO Area.

### B. Building Placement

In order to ensure that the development within the LCO complements the urban character of downtown, all structures should be located upon the property in accordance to the following guidelines:

**6.B.1:** All buildings constructed adjacent to any public street (specifically Jackson/U.S. HWY 95, Sixth Street, Legacy Avenue, and the Jackson Street Connection) within the site should be located within five (5) feet of the front and/or street side property lines as illustrated in Figure 6.1.

**6.B.2:** Portions of buildings may be setback a maximum of fifteen (15) feet from the build-to-lines for no more than twenty (20%) percent of the total building frontage length to allow for façade articulation and the creation of public space amenities that are deemed by the hearing body to provide public benefit.

**6.B.3:** Minor façade articulations of less than three feet in depth should be considered as being placed upon the build-to-line.

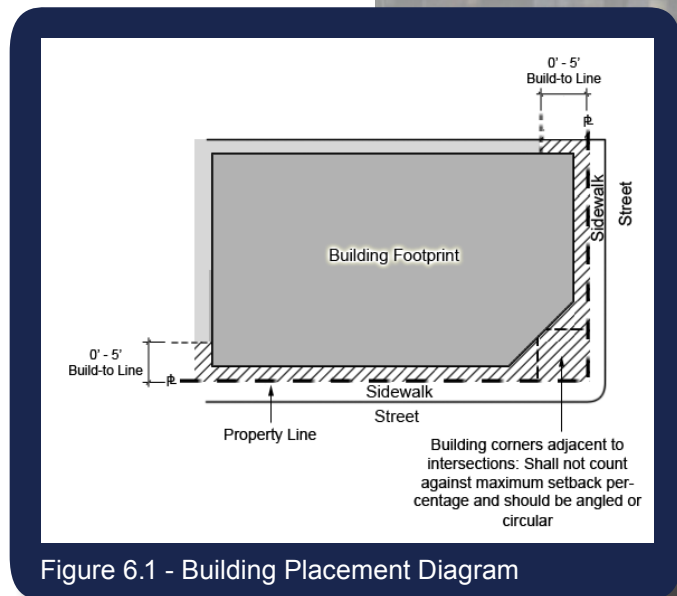






Figure 6.2 - Example Corner Treatment

**6.B.4:** Building setbacks from the build-to-line at intersection corners that are necessary to preserve visual sight distances are not to be counted against the maximum setback percentage and should be accomplished via angled or other building corner treatments to provide adequate motorist and pedestrian visibility as illustrated in Figure 6.2.

### C. Building Height

**6.C.1:** The minimum building height within the LCO should be twenty (20') feet or a minimum of two (2) stories. If an applicant proposes to construct a single story structure, the building parapet wall heights shall be increased to provide massing and appearance similar to that of a two story structure.

**6.C.2:** The maximum height at the build-to-line should be sixty (60) feet or five (5) stories. Any portion of a structure above such height should be stepped-back fifteen (15) feet from any property line adjacent to a public street or pedestrian corridor to ensure that the structure retains a pedestrian scale as illustrated in Figure 6.3.

**6.C.3:** The approving body may allow deviations from the minimum and maximum building heights if deemed justified by the character or nature of the proposed development.



Figure 6.3 - Example Desired Building Height and Step Back



## D. Building Design

### General Building

#### Façade Design:

Building façade design should comply with the following guidelines:

**6.D.1:** The front facade of all buildings should include the main entrance to the building and a number of transparent window openings.

**6.D.2:** Blank walls on all facades that front a park, street, plaza, or other public spaces should be discouraged.

**6.D.3:** On all facades, a clear visual distinction between each floor should be provided.

**6.D.4:** Facades should include both solid surfaces and window openings to avoid the creation of blank walls and/or glass curtain walls. Window openings should be balanced to avoid large blank wall surfaces on the façade.

**6.D.5:** Rear and side facades should be designed with similar architectural elements, materials, and colors as the front façade. However, the design of side and rear facades may be simpler, and more casual in nature.

#### Window Openings:

Window openings should be designed as follows:

**6.D.7:** Fifty (50%) to eighty five (85%) percent of ground-floor



Figure 6.4 - Projecting Façade Element Example





façades that front public streets and pedestrian ways should be occupied by windows and/or entry doors.

**6.D.8:** Windows and doors should utilize clear transparent glass in order to provide clear views of storefront displays from the street and to allow natural surveillance of the street and adjacent outdoor spaces.

**6.D.9:** Mirror and tinted glass should be discouraged. Moderately tinted glass for energy conservation should be allowed.

**6.D.10:** For all floors above the ground floor, windows should occupy at least twenty (20%) of the total wall areas and should have a vertical orientation and proportion.

### Projecting Façade Elements:

Projecting façade elements that provide shade and shelter to pedestrians are encouraged as follows:

**6.D.11:** Projecting façade elements should be in keeping with the historic architecture within the community including awnings and marquees.

**6.D.12:** Where buildings are adjacent to public streets or pedestrian ways, at least 75 percent of the façade width should contain projecting façade elements that provide shade and shelter to pedestrians as illustrated in Figure 6.4.

### Façade Articulation:

Street facing building façades, as well as all façades that front a plaza or pedestrian way should be articulated to improve the quality of the design.

**6.D.13:** Appropriate methods of articulation include, but are not



Figure 6.5 - Façade Articulation Illustration



limited to, the following (as illustrated in Figure 6.5):

1. Increasing the number and/or size of window openings;
2. Creating a defined base for the building;
3. Recessing storefronts, windows and/or entry ways to create depth and cast shadow patterns;
4. Providing three-dimensional expression lines (vertical and horizontal) between the floors of the structure and around storefronts and window openings; and
5. Adding depth and detail to the cornice or roof parapet.

## Building Materials:

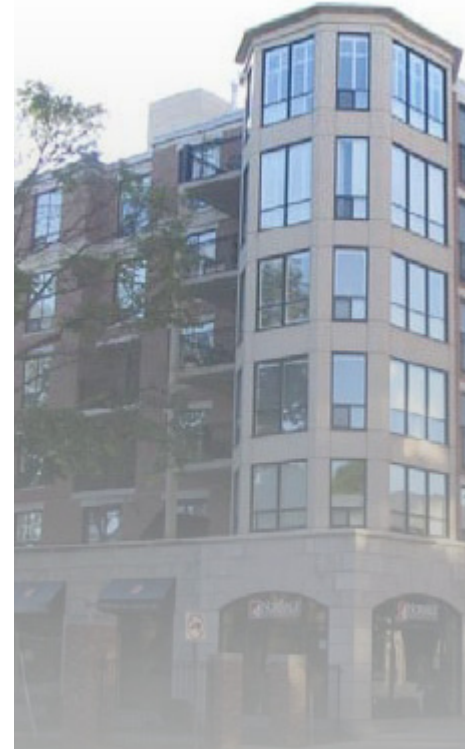
**6.D.14:** Appropriate building materials that are encouraged on facades include:

- Rock, stone, or tile
- Brick
- Plaster or stucco
- Concrete or masonry
- Finished and painted horizontal and vertical wood siding
- Fiber cement siding
- Wrought iron
- Finished and painted wood trim
- Wood, aluminum, copper, steel, and vinyl clad wood frames for windows and doors
- Wood, metal, and glass doors
- Other materials of similar quality, durability, and character to those listed above as approved by the approving body.

**6.D.15:** Inappropriate building materials that are discouraged on facades include:

- Plywood
- Hardboard
- Unfinished lumber
- Corrugated fiberglass
- Vinyl siding
- Sheet metal or tin siding other than for minor detail use

**6.D.16:** Painted building surfaces should have a matte or low-gloss finish. Trim work may have a glossy finish.



# Parking

## 7

### A. Introduction

While the City has taken great efforts to promote the use of non-automotive modes of transportation within the community, and certainly within the development of the LCO area, it is still necessary to make reasonable accommodations to provide vehicular parking to service the ultimate development of properties within the LCO area.

In order to provide for the greatest flexibility in serving the varying parking demands that are anticipated to occur with the desired mixed-use development within the LCO, the development and provision of public parking within the LCO is encouraged.

It is anticipated that the majority of this public parking (estimated as 350 potential parking stalls) will be provided via on-street public parking dispersed throughout the LCO area along Legacy Avenue and the Jackson Street Connection and frontage improvements along Sixth Street and College Street.

### B. Off-Street Parking Requirements

**7.B.1:** Vehicular parking should be required for developments within the LCO area provided in accordance with the following standards:

Use of Building or Site	Minimum Number of Parking Spaces Required
Residential Uses	1 Stall per Dwelling Unit
Hotel	1 Stall per Room
Convention/Meeting Space	1 per 10 fixed seats and 1 per each 100 sq. ft. of assembly space without fixed seating
Office, Retail, and Other Uses	No minimum parking specified if provisions for the creation of public parking are provided through the dedication and improvement of public streets in accordance with this plan



## C. Off-Street Parking Placement

**7.C.1:** Off-street parking facilities should not be located between structures and adjacent public right-of-ways or pedestrian corridors.

**7.C.2:** Off-street parking should be located underneath, behind, or to the side of all principle structures, or within parking structures.

**7.C.3:** Other than public parking facilities, surface parking lots should be minimized in order to reduce the detrimental impacts of large surface parking lots within the LCO area.

**7.C.4:** Where podium parking is provided it should be screened from public view by the building with habitable space or other building features.

## D. Off-Street Parking Joint Use Provisions

Shared joint use of off-street parking facilities among multiple properties is encouraged and may be approved by the approving body.

**7.D.1:** Any proposal of shared joint use must include a detailed parking demand analysis including projected timing and volume of parking demand generated by the proposed uses and other relevant information deemed necessary to assess the joint-use request.

## E. Off-Street Surface Parking Landscape Guidelines

Where off-street surface parking is provided, such parking areas should be adequately landscaped to visually buffer the vehicular parking area from adjacent streets and provide summer shading of the area.

**7.E.1:** Surface parking areas should include one canopy tree placed within or around the perimeter of the parking area for each five (5) stalls provided.

**7.E.2:** Off-street surface parking areas should be screened from adjacent public streets or pedestrian corridors by a five (5) foot wide landscape strip with shrubs or similar plantings that will provide adequate visual screening of the area at maturity and planted four (4) feet on center. The landscape strip may be reduced to three (3) feet in width with the inclusion of a fence or wall used for screening purposes.





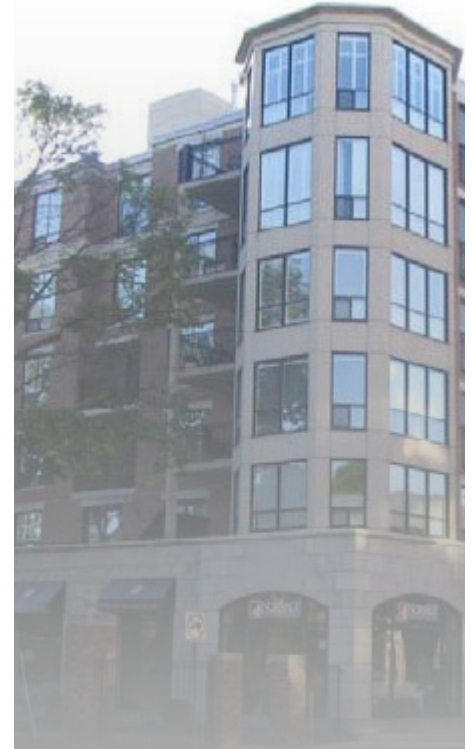
## F. Parking Structure Design Guidelines

While parking structures are a preferred method for the provision of off-street parking within the LCO area, care must be taken to ensure that the visual appearance and scale of such facilities are in keeping with the intended character of the LCO area. Any proposed parking structure within the LCO area should comply with the following:

**7.F.1:** The exterior design of parking structures should minimize its visual identity as parking through façade articulation, window openings and variations in color, material and/or texture.

**7.F.2:** Where possible, parking structures should be placed in a manner to minimize interruption of street frontage and screened from view by habitable space of the uses that they serve.

**7.F.3:** Where parking structures are located adjacent to public streets or pedestrian areas, the exterior of the parking structure should include architectural detail such as decorative façade features, planters, and storefronts to minimize the appearance of the parking structure to motorists and pedestrians.



# Public Spaces and Amenities

## 8

### A. Introduction

Integration of public spaces and similar amenities are vital in creating a sense of place and enhancing the social interaction and wellbeing of a neighborhood and community.

### B. Key Public Spaces

**8.B.1:** Developments within the LCO area should incorporate public open spaces and features (such as plazas, fountains, seating and gathering areas and pocket parks) as key focal points within the LCO area as identified within Figure 8.1 and Appendix C.

### C. Natural and Riparian Areas

The LCO area includes a portion of Paradise Creek and its associated riparian and floodplain areas. It is anticipated that the enhancement of these riparian areas will provide a recreational and visual amenities to the LCO area that should be integrated within adjacent developments.

**8.C.1:** Development proposals should provide for the protection of these sensitive areas and incorporate their natural beauty within the development to preserve these areas, minimize property damage from flooding and provide for passive recreational opportunities.

### D. Public Art Integration and Key Installation Locations

Public art and cultural amenities build artistic and creative energy



Figure 8.1 - Key Public Art Installation Locations



within the community and enhance economic and social vitality. Public art installations create a sense of place and promote cultural tourism as an economic industry.



Figure 8.2 - Public Art Example

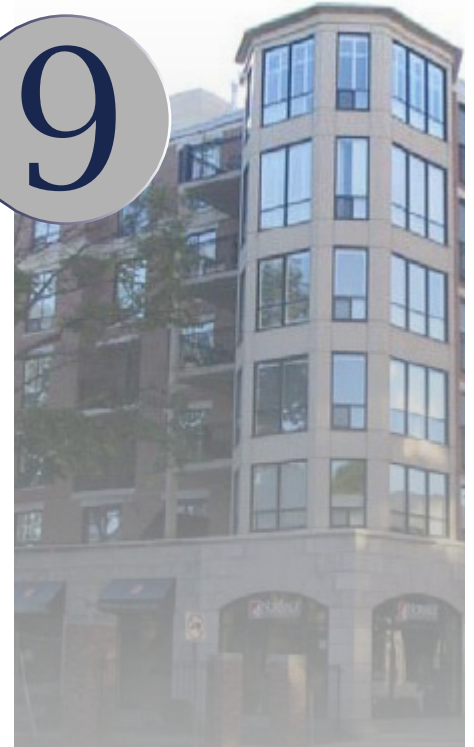
**8.D.1:** Public art incorporation within building facades, street furnishings, public installations and other development components are highly encouraged.

**8.D.2:** Developments should incorporate public art installations within all proposed key public spaces and the identified installation locations identified in Figure 8.1 and Appendix C.



# Sustainable Development Practices

## 9



### A. Stormwater Management

**9.A.1:** To the greatest extent possible all developments within the LCO area should utilize best management practices to capture, detain, reuse, treat and otherwise control and reduce stormwater runoff from the development site. Such management plans should include items such as pervious pavement materials, bioswales, green roofs, rainwater harvesting, and other such practices.

**9.A.2:** All developments within the LCO area should integrate low impact stormwater management practices in addition to meeting all other stormwater requirements and provisions of Moscow City Code.

### B. Public Transit Facilities

Developments within the LCO area should provide for the logical location and distribution of public transit facilities to promote greater utilization of public transit services in the area and reduce automobile parking demand.

**9.B.1:** All developments within the LCO area should assess the need for transit facilities necessary to serve the development including the location of public transit stops, benches, shelters and signage.

### C. Bicycle Parking Facilities

Developments within the LCO area should provide for the logical location and distribution of bicycle parking facilities to promote greater utilization of bicycles as a mode of transportation to and from the area and reduce automobile parking demand within the area.

**9.C.1:** Developments within the LCO area should provide bicycle parking for each use of a lot or structure in accordance with the following standards:



Use of Building or Site	Minimum Number of Parking Spaces Required
Residential Uses	1 Bicycle Space per Each Two (2) Dwelling Units
Hotel	1 Bicycle Space per Each Ten (10) Rooms
Convention/Meeting Space	1 Bicycle Space for Each Ten (10) Automobile Parking Spaces Provided
Office, Retail, and Similar Uses	1 Bicycle Space for Each 1,000 sq. ft.

**9.C.2:** Each bicycle parking space shall be sufficient to accommodate a bicycle at least six (6) feet in length and two feet wide, and shall be provided with some form of stable frame permanently anchored to a foundation to which a bicycle frame and both wheels may be conveniently secured using a chain and padlock, locker, or other storage facilities which are convenient for storage and are reasonably secure from theft and vandalism.

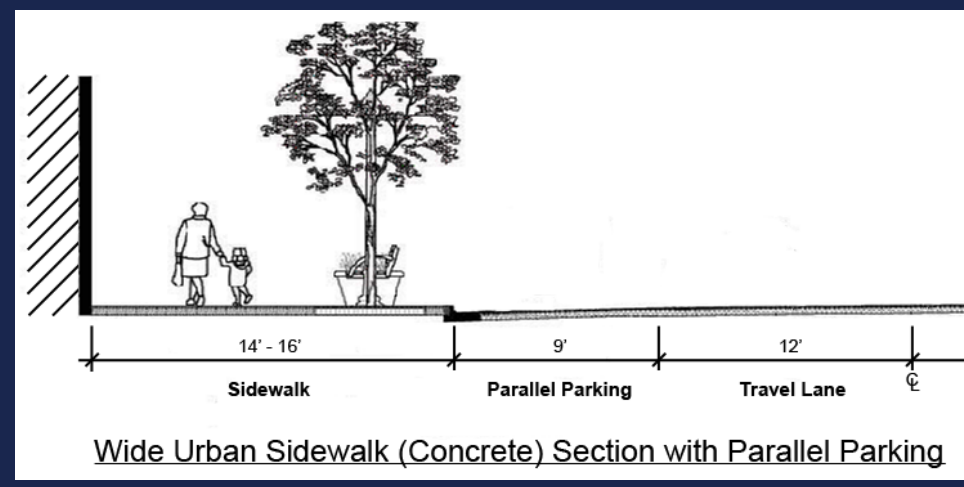
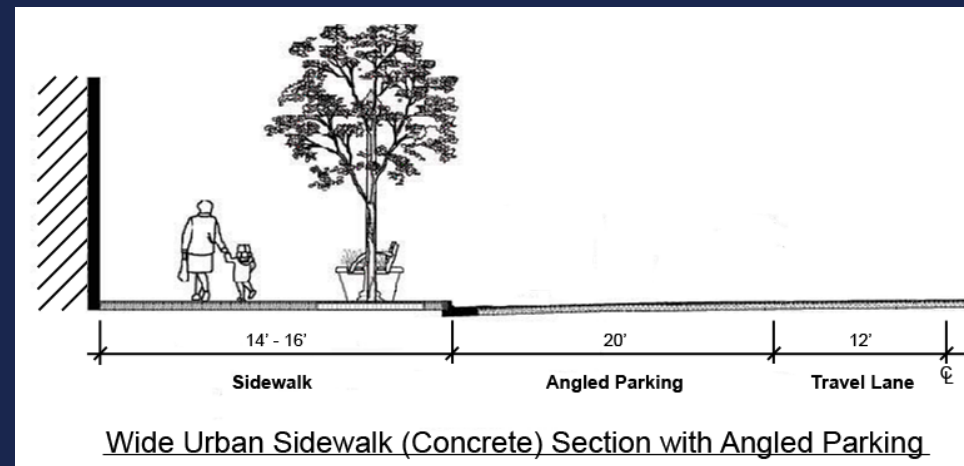
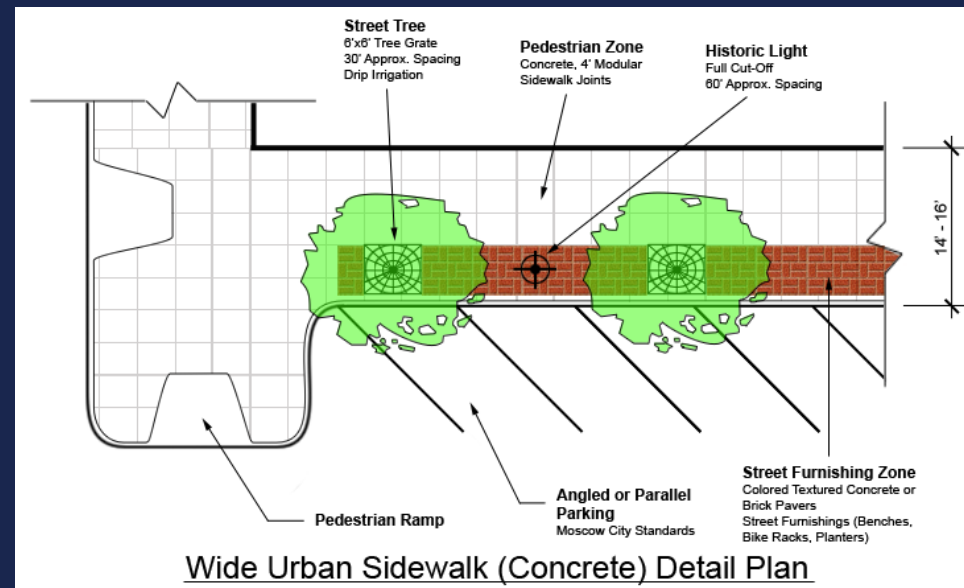
**9.C.3:** The separation of the bicycle parking spaces and the amount of corridor space must be adequate for convenient access to every space when the parking facility is full.

**9.C.4:** When automobile parking spaces are provided in a structure, all required bicycle spaces shall be located inside that structure or should be located in other areas protected from the weather. Bicycle parking spaces in parking structures should be clearly marked as such and should be separated from auto parking by some form of barrier to minimize the possibility of damage from a vehicle.





# Site Ingress & Egress

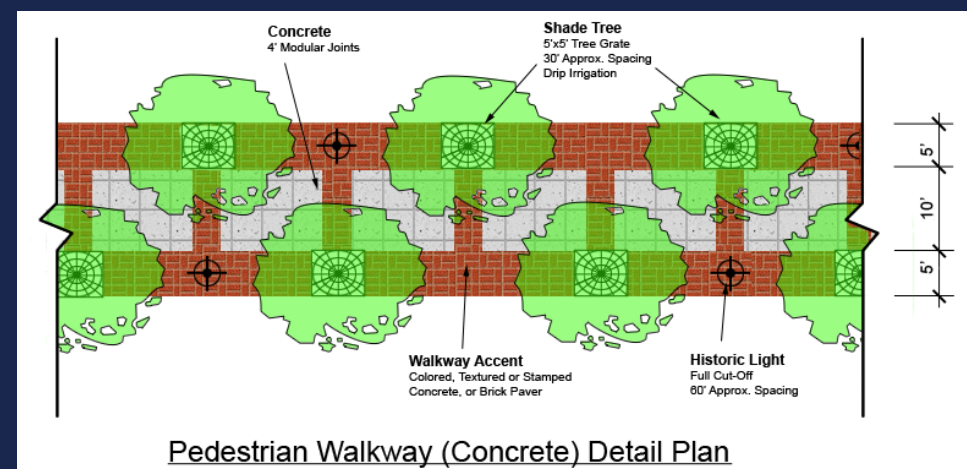
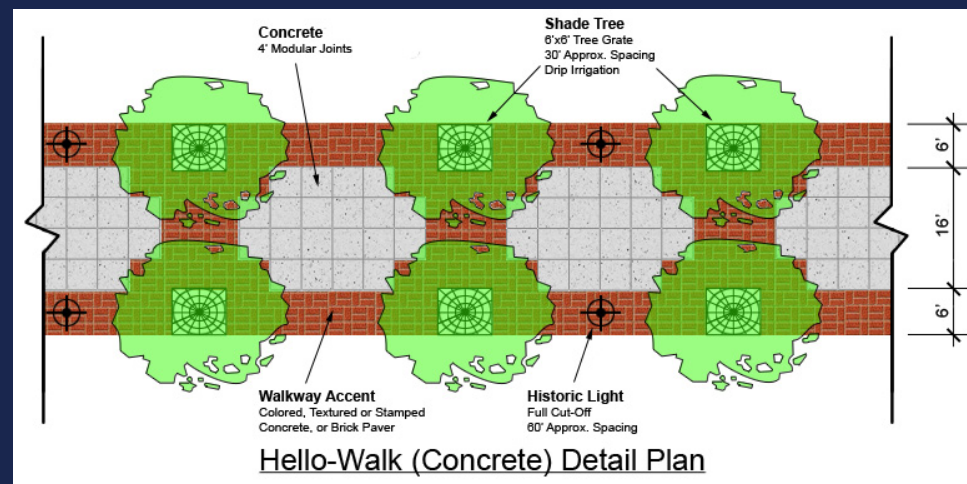
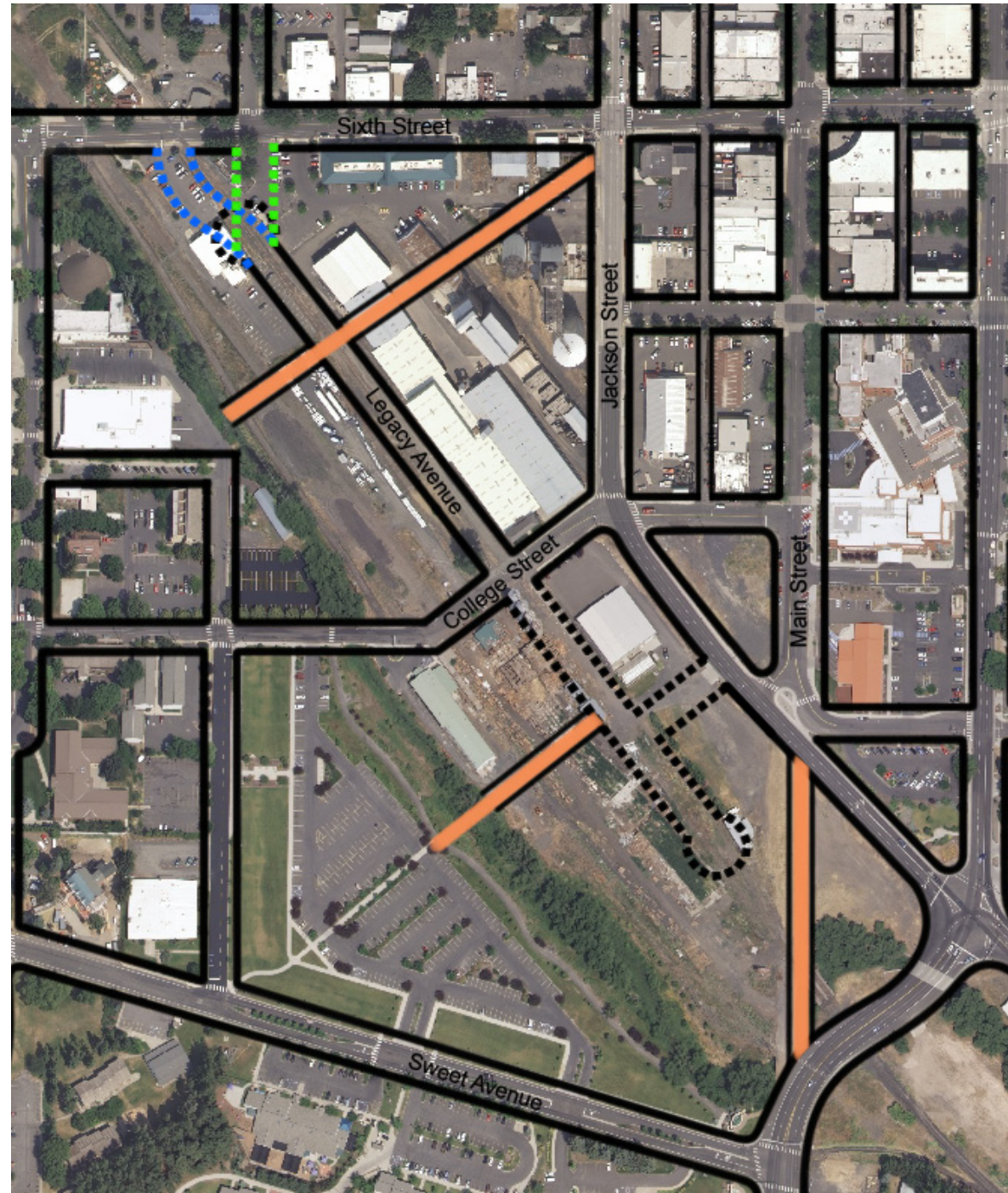


Appendix A

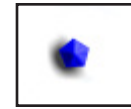
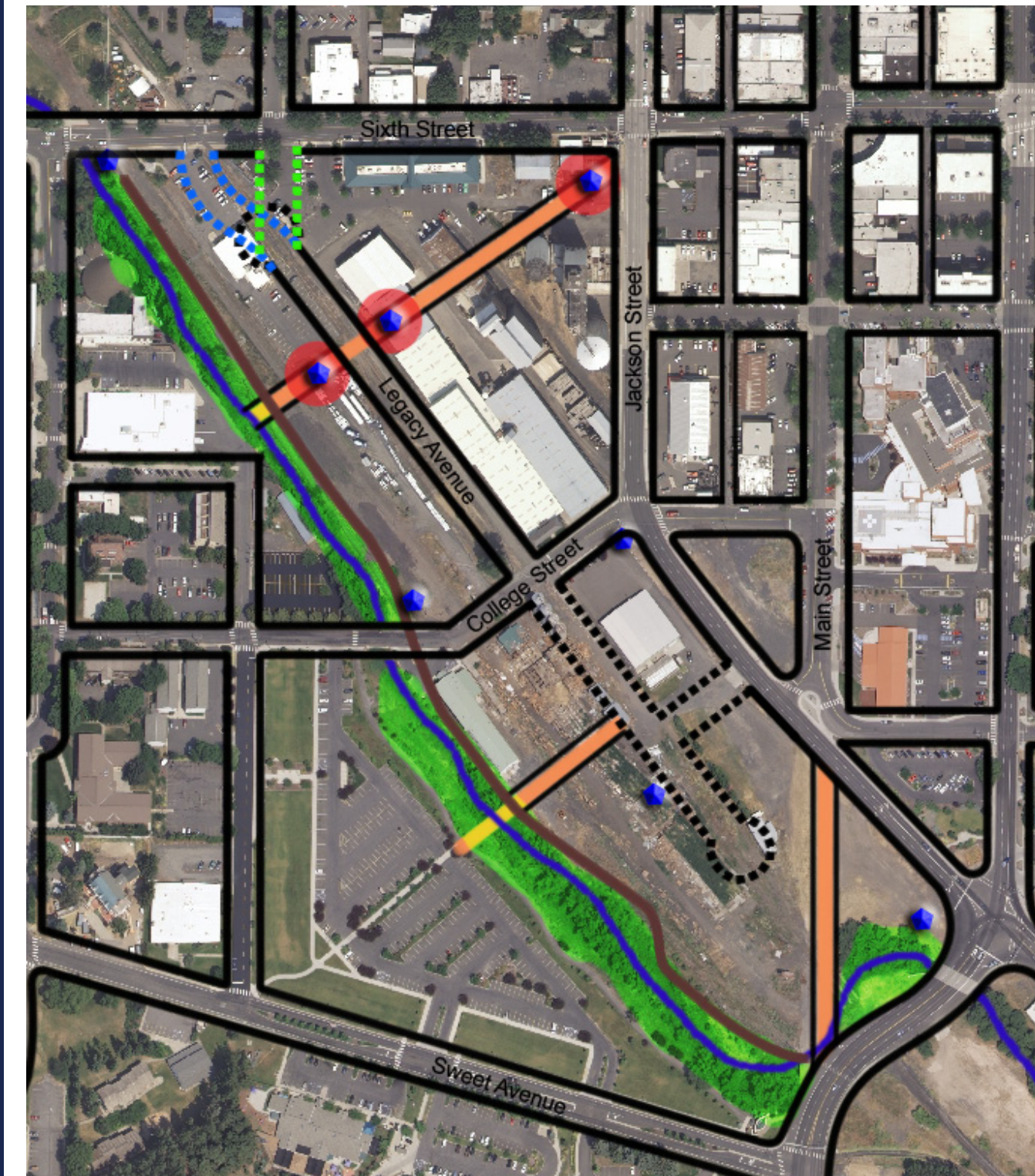




# Pedestrian Corridor Facilities







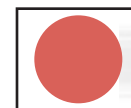
Public Art Installation



Riparian Area



Pedestrian Pathway



Public Spaces







MOSCOW  
**Urban Renewal Agency**  
Strategic Plan 2020-2025

## MOSCOW URBAN RENEWAL AGENCY

Founded in 1995 to promote community and economic development, the Moscow Urban Renewal Agency operates under Idaho Code in accordance with Idaho Urban Renewal Law and the Local Economic Development Act. The Agency's purpose is to undertake the rehabilitation, conservation, development or redevelopment of areas identified by the City as being in the best interest of the public health, safety, morals or welfare of its residents.

Agency Commissioners are appointed by the Mayor and confirmed by the City Council, with terms as authorized by Moscow City Council Resolution 2008-17. Two members are appointed from the Moscow City Council, one member from the Latah County Board of Commissioners, and four from the citizenry at large. Terms are staggered such that no more than three members expire in any given year.

### The Board

The Board of Commissioners elects the Chair, Vice-Chair and Secretary from the ranks of the Commission annually; the Treasurer office may be filled by Commissioners or by staff appointments made by the Commission. The City of Moscow provides ongoing staff support to assist with operational needs of the Agency.



#### *2019 MURA Board (pictured from left to right)*

Steve McGeehan, Chair

Brandy Sullivan, Vice Chair

Art Bettge, Secretary

Steve Drown, Commissioner

Dave McGraw, Commissioner

Nancy Tribble, Commissioner

Mark Beauchamp, Commissioner (not pictured)



## Mission

The Mission of the Agency is to promote sustainable economic **growth**, vitality, and community **enhancement** through **collaboration** and community **investment**.

<b>GROWTH</b>	Grow the local economy to increase community vitality, resilience, and strength
<b>ENHANCEMENT</b>	Enhance and contribute to community assets that make Moscow a great place to live, work, and play
<b>COLLABORATION</b>	Collaborate with partner agencies and others to achieve identified community needs and aspirations
<b>INVESTMENT</b>	Invest in the built, natural, social, and cultural environments to foster and promote sustainable community and economic growth

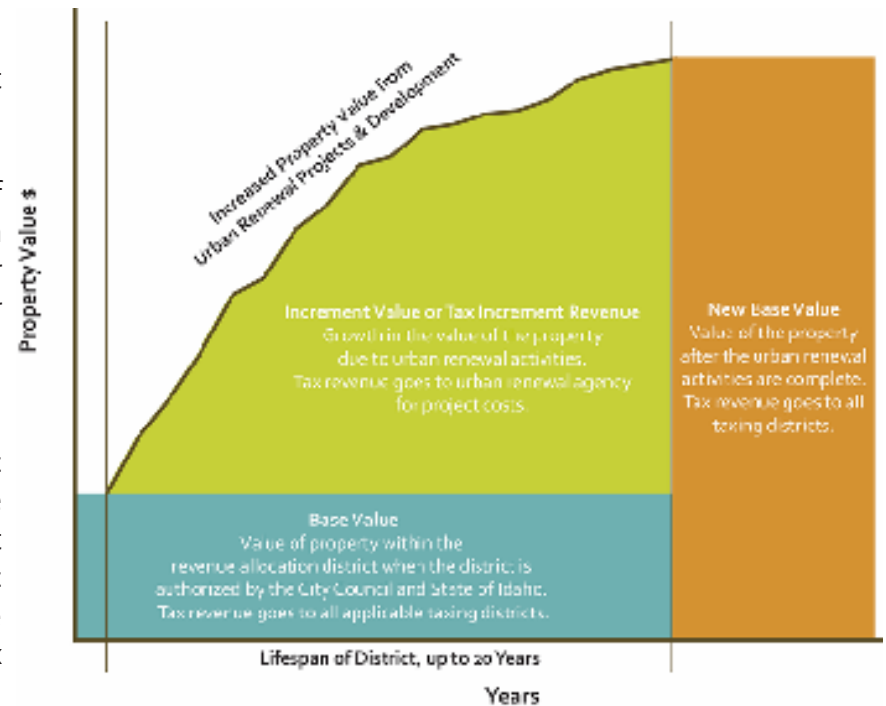
## Vision

The Agency serves as a valuable, effective, and responsive community partner working to strengthen and enhance Moscow’s physical, cultural, and economic wellbeing through strategic public investment that supports the community’s vision and desires.

## History and Financing

Urban renewal and revenue allocation financing are the most significant tools available to Idaho communities for attracting and retaining businesses, generating economic development, promoting job creation and encouraging development of deteriorating and underutilized areas. Revenue allocation financing allows communities to make a site ready for development, such as extending water, sewer, streets and other improvements that reduce the cost to businesses of relocating or expanding.

The City of Moscow creates and defines the geographic boundaries of urban renewal districts within the city. The Agency works with the City and private sector to remedy blight and to facilitate economic development as directed by specific urban renewal plans adopted by the Moscow City Council. The Agency provides funding for these efforts through the use of tax incremental financing.



As illustrated in the graphic on the preceding page, when the city establishes a tax increment financing district, the value on the property in the district is set as of the date the district is created. The property tax revenue collected on this base value goes to the various taxing entities providing services to that property. Any increase in value over the base is called the increment value and the tax revenue generated from the increment value is transferred to the Agency. These tax increment revenues are used by the Agency to pay for public improvements and other revitalization activities in that district. When the district closes (currently 20 years from creation) the increment value is added back to the base value on the tax rolls. This helps diversify and strengthen the economic bases of both the city and the county. Although urban renewal is a separate item on property tax statements, local property owners pay the same amount of tax whether or not an urban renewal district is established in their area.

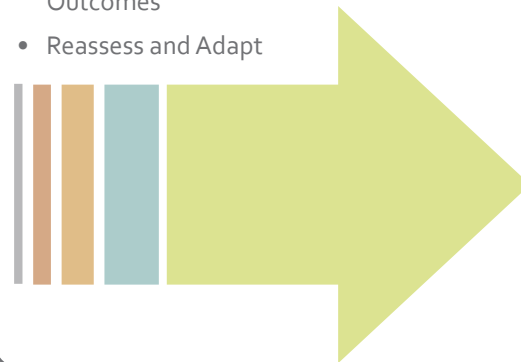
To date Moscow has identified two urban renewal districts: the Alturas Technology Park District created in 1995, and the Legacy Crossing District created in 2008. The Alturas Technology and Business Park District closed in 2015 but the Agency continues to hold six lots within the District that for sale by the Agency. In addition, the City has stated a desire to explore a third district to support new industrial development within Moscow, and to amend the Legacy Crossing District boundary to include the adjacent portion of Main Street.

## STRATEGIC AND CAPITAL IMPROVEMENT PLANS

This document includes both strategic and capital planning elements to identify and articulate the community goals and aspirations, as well as the public investment that is intended to achieve these outcomes. Strategic and capital planning work hand in hand to identify and articulate organization goals and objectives and provide the budgetary framework necessary to achieve them.

### STRATEGIC PLANNING

- Establish Organizational Mission, Vision, and Values
- Identify Desired Community Outcomes
- Establish Strategies to Achieve Desired Outcomes
- Align Partner Agency Efforts and Resources
- Measure Progress Toward Desired Outcomes
- Reassess and Adapt





Strategic planning formalizes desired community goals and objectives as identified by partner agencies and aligns MURA efforts and resources toward achieving these community outcomes. As a continuing and on-going process, the MURA and its partner agencies will utilize the strategic planning cycle to measure the effectiveness of the strategies employed, and reassess and adjust these strategies as necessary to meet changing community needs and desires.

The Capital Improvement Plan (CIP) identifies capital projects over the short-range planning horizon. The CIP forms the foundation of the budgeting process and serves as the financial planning framework to guide and direct current and future community investment.

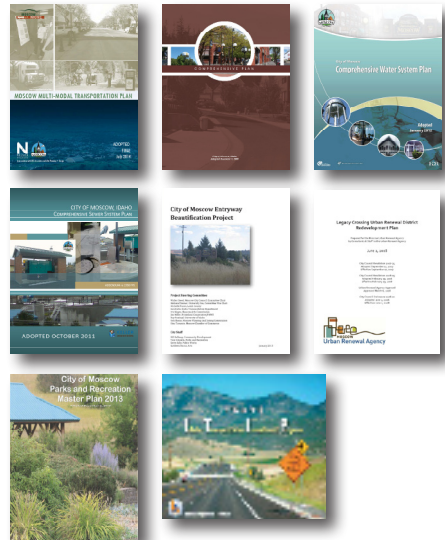


## PARTNER ENTITIES

Partner agencies provide community vision and identify community needs and desires within the Districts operated by the Agency. These common interests present opportunities for the MURA to collaborate with its partner agencies to leverage limited Agency resources into important community investments. In preparation of this document, the MURA's partner entities were surveyed to understand their needs and how to improve collaboration.



**PARTNER ENTITIES**

**PARTNER PLANS**

City of Moscow  
 Latah County  
 University of Idaho  
 Gritman Medical Center  
 SMART Transit

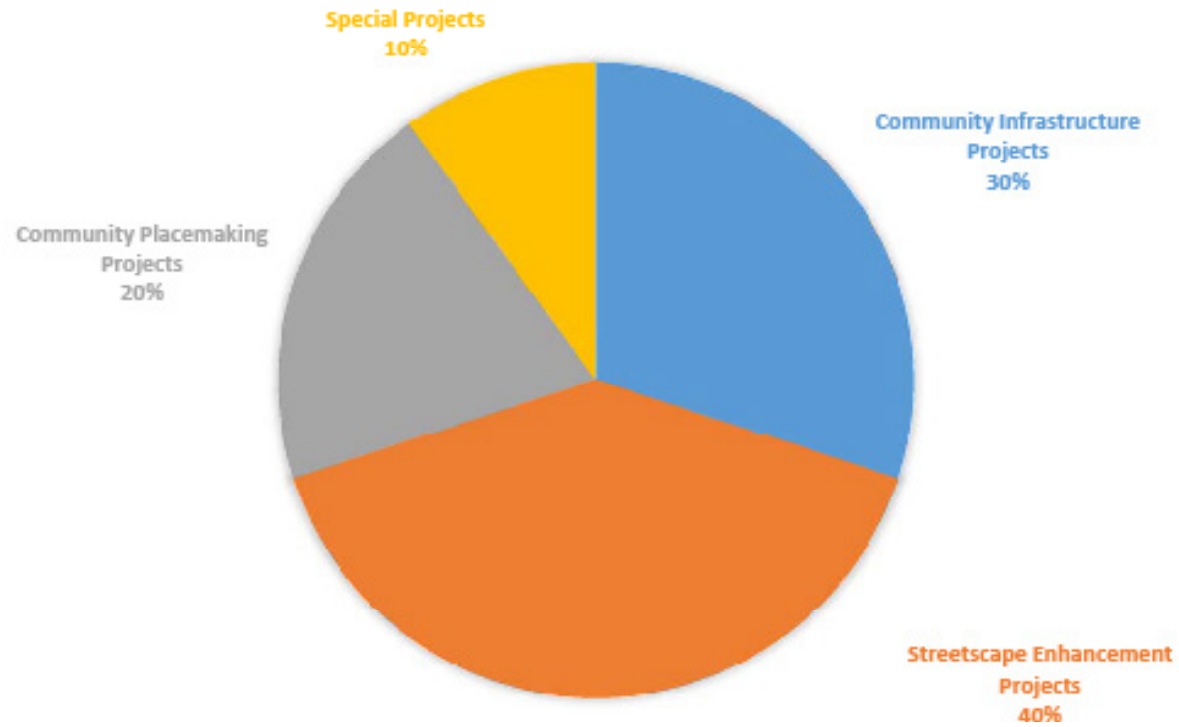
Idaho Transportation Department  
 Moscow Chamber of Commerce  
 Partnership for Economic Prosperity  
 Idaho Department of Commerce

## PUBLIC INVESTMENT PORTFOLIO

Urban renewal agencies within the State of Idaho are generally restricted to the funding of public infrastructure such as water and sewer utility extension or expansion, public roadway expansion and improvement, streetscape and sidewalk improvement, and participation in the development of community amenities such as public facilities, buildings, parks, art installations and similar improvements. Urban renewal agencies may also acquire and assemble property, and fund demolition and environmental remediation activities in support of private development and investment. The four main components within the MURA public investment portfolio are: Community Infrastructure, Streetscape Enhancements, Community Placemaking and Special Projects. All of these investment components are intended to address community needs, improve deteriorating areas, and promote private investment and reinvestment. The MURA Board has identified annual target allocations for each of the portfolio components as a general guideline for capital planning and investment while retaining flexibility to address opportunities and unforeseen needs.

Investment Portfolio Components	Portfolio Component Typical Elements	Portfolio Component Community Outcomes
<b>Community Infrastructure</b>	Public Utility (water, sewer, storm, communications) improvements and expansions, roadway extensions, expansions or improvements	To provide needed public services to support new private investment or correct a current sub-standard or deteriorating condition to encourage private investment and reinvestment
<b>Streetscape Enhancements</b>	Sidewalk construction and enhancements, ADA access improvements, public lighting, street tree, and street furnishing installations	To improve pedestrian environment and ADA compliance leading to increased pedestrian and economic activity, and to improve community appearances and aesthetics incentivizing private investment and reinvestment
<b>Community Placemaking</b>	Creation of public spaces, plazas, parks and greenspaces, public art installation and community beautification improvements	To enhance and strengthen social and cultural public environment and community aesthetics, which improve deteriorating areas and incentivize complementary private investment and reinvestment
<b>Special Projects</b>	Land acquisition and assemblage, environmental remediation, public parking facilities, community facilities and buildings, and other special projects	To facilitate property development, redevelopment, environmental remediation, improvement of public health and safety, provision of needed community facilities and buildings, and increased private investment and reinvestment

### PROPOSED INVESTMENT ANNUAL ALLOCATION MODEL

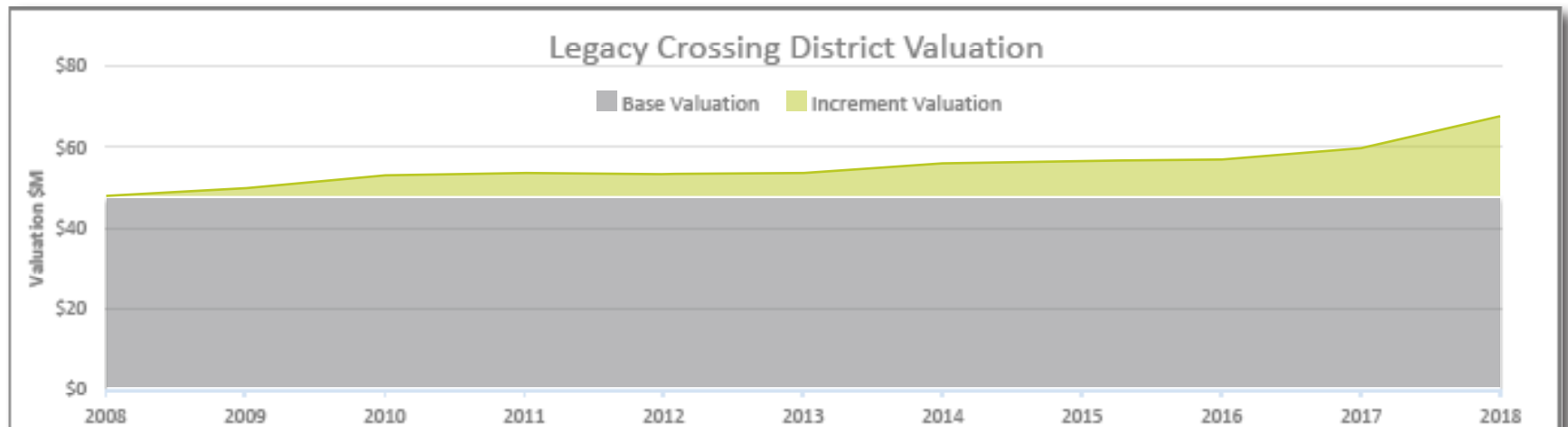
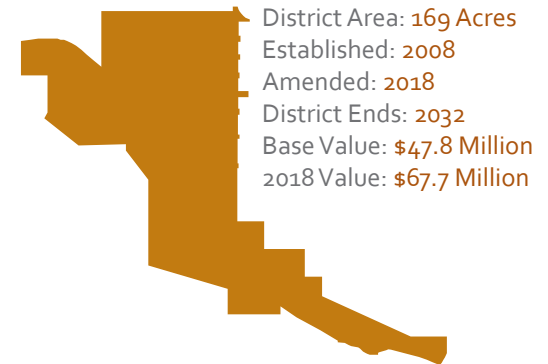




## MURA DISTRICTS

### Legacy Crossing District

The Legacy Crossing District, initially created in June of 2008, covered 163 acres including a majority of the blighted and underutilized properties located between Moscow’s historic downtown and the University of Idaho. The primary objective in creating this District was to eliminate conditions impeding the City’s economic growth in the area. The Agency’s focus is to spur more rapid land use transition of properties from former agricultural and/or industrial uses to new uses and economic vibrancy adding to the quality of civic life and improving the public safety of citizens and visitors. The Agency has participated financially in public infrastructure and environmental remediation to support a number of projects including the Anderson Silos, EMSI, Dawson’s Corner, Identity on Main Street, Varsity Diner, and Gritman Medical Office III projects. In 2017, the City Council requested the Agency’s assistance with the deteriorating public infrastructure in downtown and in 2018 the District was amended to include the adjacent 6 acres including Main Street from A Street to 8th Street, bringing the total District size to 169 acres. Continued utilization of these public-private partnerships will provide the resources necessary to redevelop the Legacy Crossing District into a vibrant and attractive mixed-use neighborhood.

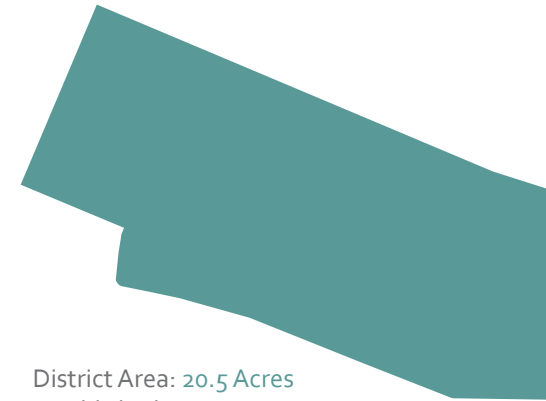


## Alturas Technology Park District

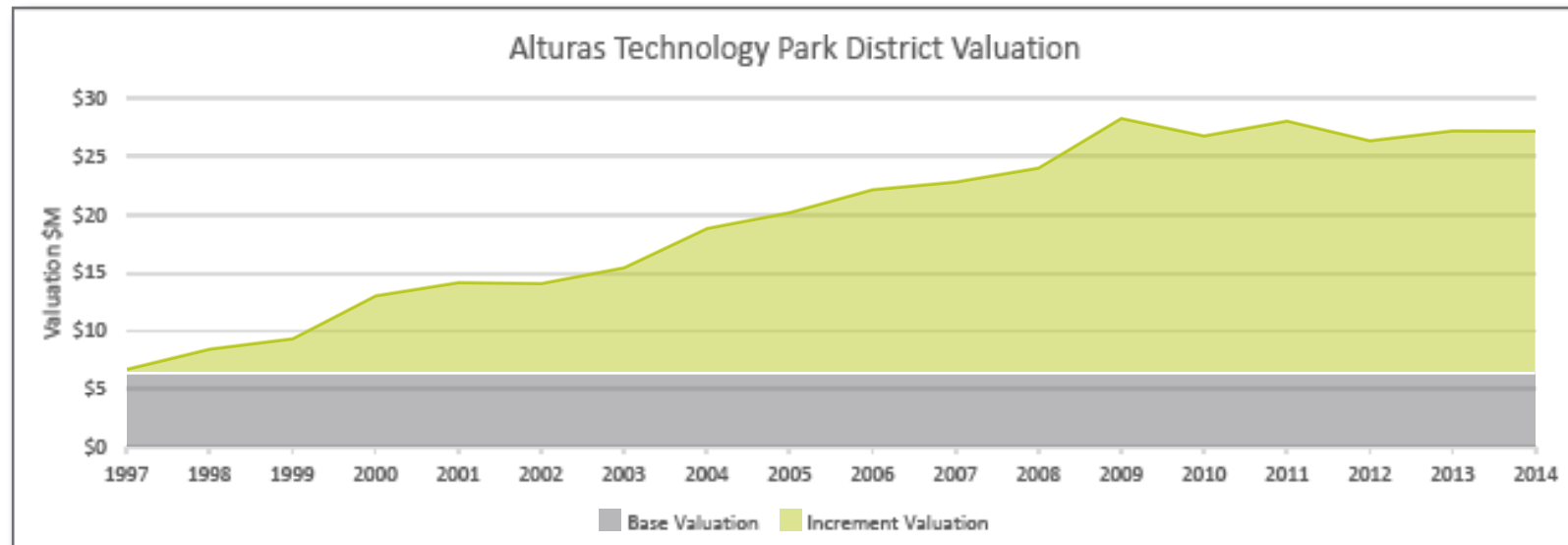
The Alturas Technology Park was the Agency's first District and is currently home to many of Moscow's premier high-tech companies including Comtech EF Data Corporation, Alturas Analytics, Inc., Anatek Labs, Inc., and BioTracking, LLC. The export industries within the Alturas Technology Park have a profound economic impact on the Moscow economy. These companies had a total payroll of over \$6 million and paid an average wage of over \$50,000, which is significantly higher than the city's median household income of \$35,389. During that period, the Park contributed an estimated adjusted impact of \$26.7M to the local community.

Established in 1996 with an assessed valuation of \$6.4 million, improvements and developments made as a result of the Alturas Research and Technology Park Urban Renewal Plan have assisted in increasing property values to more than \$27 million today. This District was closed in 2015, one year earlier than anticipated in the District Plan.

The Agency has engaged a local real estate firm to provide marketing and real estate brokerage services for the six lots remaining for sale within the Park.



District Area: 20.5 Acres  
 Established: 1996  
 District Closed: 2015  
 Base Value: \$6.4 Million  
 2015 Value: \$27.4 Million



# MURA STRATEGIC PLAN GOALS, OBJECTIVES AND STRATEGIES

## General Agency Goals and Objectives

General Agency goals and objectives address the overall operation of the Agency not pertaining to a specific urban renewal district. During the strategic planning process, many partner agencies identified a desire to increase collaboration and coordination with the Agency. Through this planning process, the Agency Board identified the need to improve the manner in which the Agency communicates its mission and purpose with stakeholders, partner agencies and the public to share the impacts of the public investments made by the Agency.

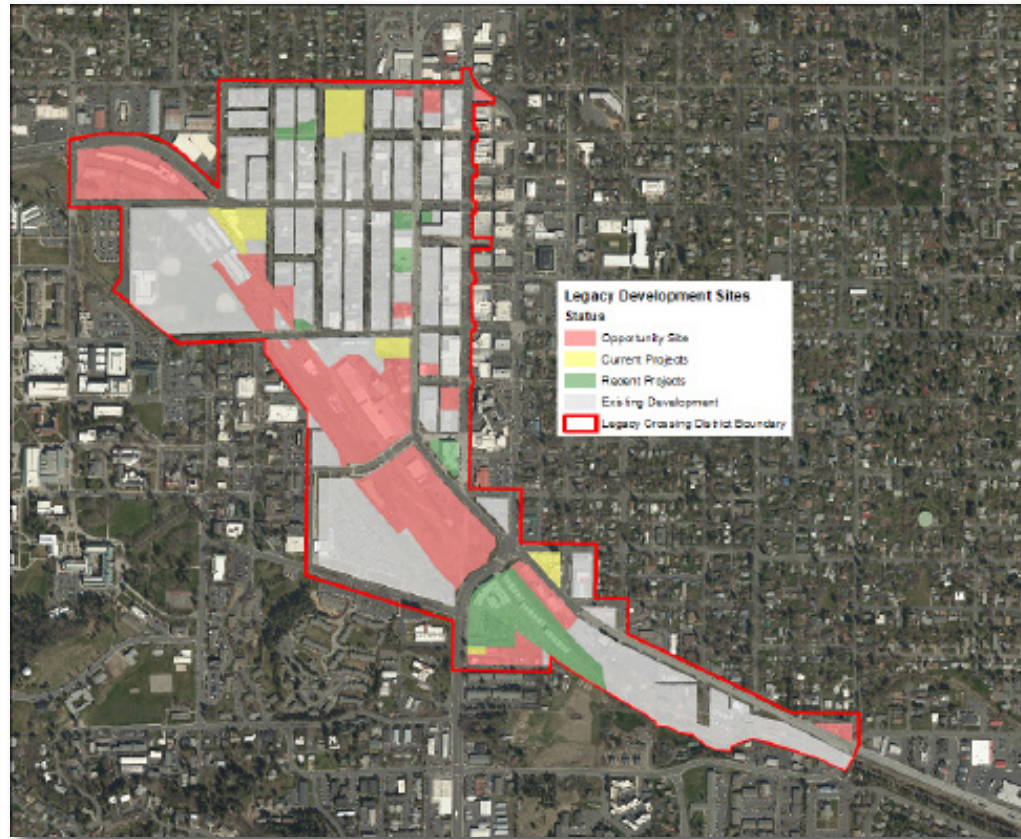
<p><b>GOALS</b></p>	<ul style="list-style-type: none"> <li>• To increase community knowledge and understanding of Agency activities and public investments</li> <li>• To increase public transparency of Agency activities and governance</li> <li>• To increase collaboration and coordination with partner entities in meeting community needs and desires</li> </ul>
<p><b>TWO-YEAR OBJECTIVES</b></p>	<ul style="list-style-type: none"> <li>• Ensure public access to information related to the activities of the Agency to promote transparency and public awareness.</li> <li>• Conduct biennial joint meetings with partner entities to increase communication, cooperation and coordination</li> <li>• Establish a new urban renewal district to assist in the City's desire to facilitate future industrial development in South Moscow</li> </ul>
<p><b>ACTIONS/ STRATEGIES</b></p>	<ul style="list-style-type: none"> <li>• Maintain the Agency's website to provide current and accurate information regarding Agency activities and community investment</li> <li>• Maintain the electronic records center repository for all Agency records to provide public access and transparency of Agency activities</li> <li>• Maintain the Agency's five-year strategic and capital improvement plan to organize Agency activities, document Agency public investments, and communicate them to the public</li> <li>• Conduct a joint meeting with the Moscow City Council by September 30, 2021</li> </ul>

## Legacy Crossing District Goals and Objectives

The primary objective for the creation of the Legacy Crossing District was to eliminate conditions impeding the City's economic growth in the area located between Moscow's historic downtown and the University of Idaho campus. The Agency's focus is to take actions targeted toward improvement of both underdeveloped properties and those properties in transition, with the intention of spurring more rapid land use transition of properties from former agricultural and/or industrial uses to new uses and economic vibrancy, thereby adding to the quality of civic life and improving the public safety of citizens and visitors.

The Legacy Crossing District was formed in 2008 at the peak of the Great Recession which dampened development activity over the first eight years of the District. The trend is changing, with \$40 million in new private investment over the last two years. In 2010, the Agency purchased a parcel within the Legacy Crossing District located at the southwestern corner of the intersection of 6th and Jackson streets. The Agency and the City of Moscow understood that the property was the keystone to provide the connection between the University and downtown and development of Hello Walk. The MURA has recently inventoried "opportunity sites" which are intended to identify potential underutilized sites that could be good candidates for redevelopment. Properties shown in pink are opportunity sites, yellow sites are currently undergoing redevelopment, and green sites are recently completed projects within the District. Public investment that facilitates redevelopment of opportunity sites is a priority for the Agency.

The City of Moscow has expressed a desire to amend the Legacy Crossing District boundary



to include the adjacent portion of Main Street. Much of its current infrastructure was constructed over 30 years ago through a Local Improvement District (LID) and, except for the Friendship Square renovation that occurred in 2006, the City has had limited ability to reinvestment in the downtown infrastructure, so much of the streetscape is in poor condition. The City believes downtown Moscow is a key asset representing Moscow’s unique character and quality of life which directly and indirectly influences business investment and economic activity. In response to the Council’s request, in 2018 the District was amended to include the adjacent 6 acres including Main Street from A Street to 8th Street bringing the total District size to 169 acres and providing an opportunity for the Agency to partner with the City in downtown improvements.

<p><b>GOALS</b></p>	<ul style="list-style-type: none"> <li>• To increase public and private investment and economic activity within the District in a manner consistent with the community’s vision</li> <li>• To accelerate the transition and redevelopment of brownfield sites and underutilized properties within the District</li> <li>• To strengthen, enhance and support the social, economic and cultural strength of downtown Moscow and the University of Idaho</li> <li>• To increase the condition and capacity of public infrastructure within the District to address sub-standard conditions and support new private investment</li> <li>• To increase the condition and appearance of public streets, sidewalks, pathways and other public spaces</li> </ul>
<p><b>TWO-YEAR OBJECTIVES</b></p>	<ul style="list-style-type: none"> <li>• Complete the disposition and development of the Sixth and Jackson property</li> <li>• Support the continued redevelopment of brownfield sites within the District</li> <li>• Assist in the development of a plan for the repair and replacement of the deteriorating infrastructure on Main Street</li> </ul>
<p><b>ACTIONS/ STRATEGIES</b></p>	<ul style="list-style-type: none"> <li>• Complete the exclusive negotiation process and execute the Disposition and Development Agreement for the Sixth and Jackson property by January 31, 2020</li> <li>• Assist with the development of a streetscape improvement plan for Main Street in the downtown area by December 31, 2020</li> <li>• Work with the City of Moscow to develop a consolidated Capital Improvement Plan for necessary public infrastructure improvements identified within the Legacy Crossing Plan by December 31, 2020</li> </ul>



## Alturas District Goals and Objectives

Many of the Agency's goals and objectives for the Alturas District were achieved through the development of Alturas Technology Park and the private investment that has occurred within the District. The Agency was able to close the district ahead of schedule which allowed the full value of the District to return to the tax rolls. While the District is now closed, the Agency continues to hold six lots within the Alturas Technology Park which continue to provide an asset that can be utilized to support economic development within Moscow.

### GOALS

- To increase the private investment within Alturas Technology Park
- To promote primary job creation within Alturas Technology Park
- To transition Agency-owned lots to private use and development

### TWO-YEAR OBJECTIVES

- Maintain real estate marketing and listing services to market and promote the sale of Alturas Lots
- Collaborate with regional and state economic development organizations to identify and promote development opportunities for Alturas Technology Park

### ACTIONS/ STRATEGIES

- Complete an assessment of the current Phase II private restrictive covenants and restrictions with existing Phase II property owners to identify opportunities to expand appropriate allowable uses by October 1, 2020
- Collaborate with the newly formed Partnership for Economic Prosperity (PEP) economic development organization to develop a strategy to market and recruit appropriate businesses to Alturas Technology Park by July 1, 2020



## PROJECT HIGHLIGHTS

### Downtown Public Restroom

The growing number of events and activities occurring in downtown Moscow over the last decade highlighted the need for public restrooms downtown. The Moscow Urban Renewal Agency partnered with the City of Moscow to construct an attractive public restroom facility near Friendship Square. When the City was faced with the potential need to remove the brick façade from the project to reduce cost, the Agency contributed \$27,000 toward the project to ensure the building was in keeping with the historic character of downtown Moscow.

- Project Type: **Public Facility**
- Project Valuation: **\$205,500**
- Agency Contribution: **\$27,000**
- Agency Funded Elements: **Brick Façade Enhancements**

Downtown Restroom

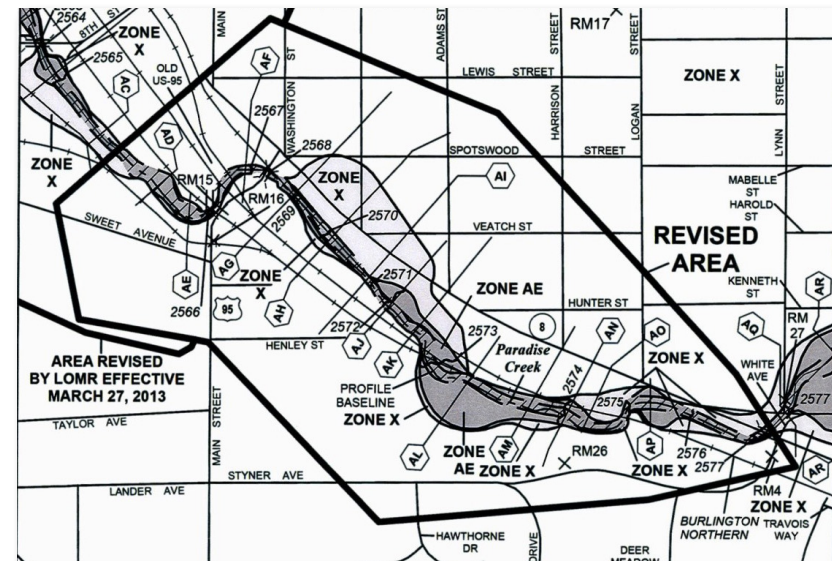
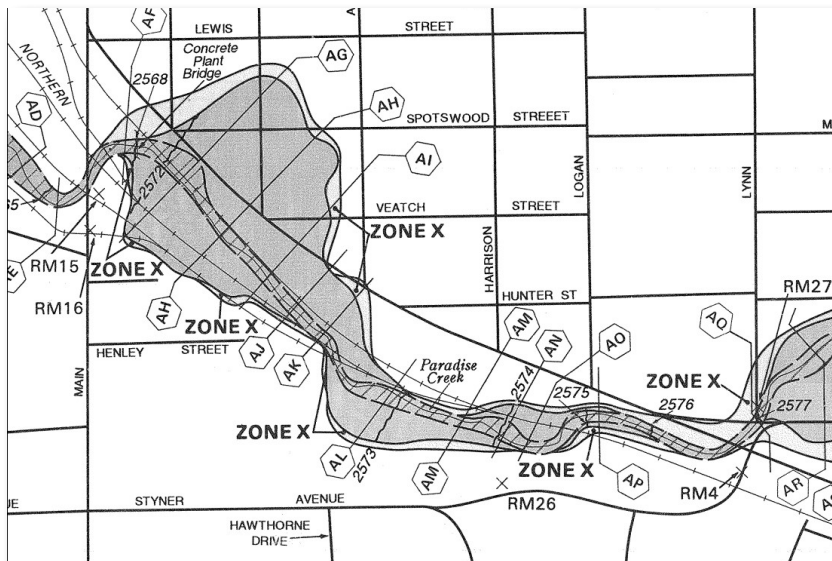


## Paradise Creek Floodplain Study

Over 27 properties located in the southeast portion of the Legacy Crossing District were impacted by inaccurate historical floodplain boundaries that were the result of a private bridge that had been removed many years ago. This situation created an impediment to redevelopment and presented a hardship for property owners to carry unnecessary flood insurance. The Agency partnered with the City of Moscow to fund a flood study and to secure a Letter of Map Revision from the Federal Emergency Management Agency removing these properties from the floodplain and allowing the redevelopment of a prior dilapidated property into the new Washington Trust building.

- Project Type: **Special Study**
- Project Valuation: **\$21,700**
- Agency Contribution: **\$10,850**
- Agency Funded Elements:  
**Floodplain Study**

Floodplain Study





## State Highway 8 Pedestrian Underpass Project

The Paradise Path is the City's most significant pedestrian pathway connecting the Latah Trail to the Chipman Trail through the City. In eastern Moscow the pathway is located on the south side of the Troy Highway, which creates a significant access impediment to the trail for residents on the north side of the highway. Following a study which determined the feasibility of constructing a pedestrian underpass beneath the existing bridge structure located at the Styner/White intersection with the Troy Highway, the City sought to apply for a grant to fund the underpass construction. The Agency contributed \$15,000 to the project which was one-half of the required local match for the grant. The underpass was constructed in 2018 and now provides pedestrians and cyclists with safe access to the Paradise Path.

- Project Type: **Pedestrian Facility**
- Project Valuation: **\$457,000**
- Agency Contribution: **\$15,000**
- Agency Funded Elements: **50% of Project Local Match**

Pedestrian Underpass



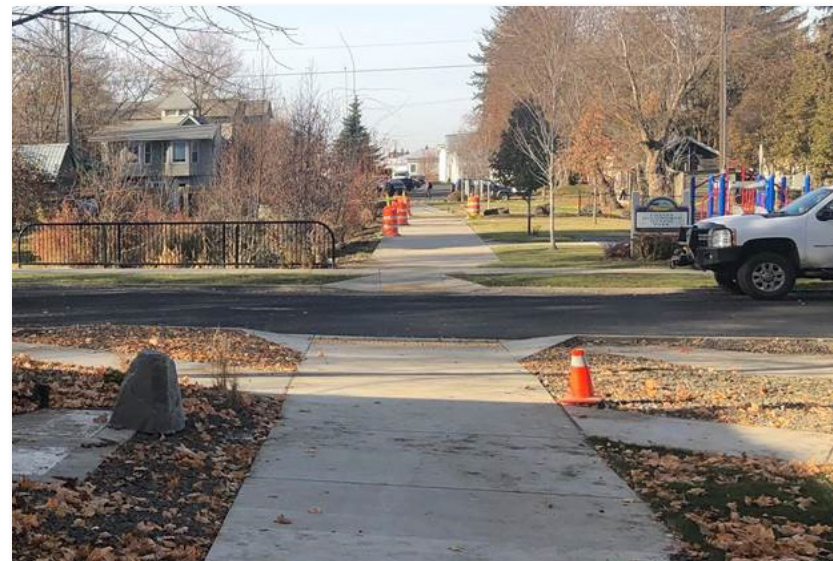


## Barley Flats Pedestrian Pathway Project

When local developer Noel Blum acquired the prior Dumas Seed property, the City of Moscow and Urban Renewal Agency partnered with Mr. Blum to secure a pedestrian easement and construct a pedestrian pathway along the south side of the property to provide pedestrian access across the site to connect Lillian Woodworth Otness Park with downtown Moscow. Mr. Blum granted the easement for the pathway and the Agency funded the pathway construction including pathway lighting.

- Project Type: **Pedestrian Facility**
- Project Valuation: **\$58,646**
- Agency Contribution: **\$58,646**
- Agency Funded Elements:  
**Pathway Construction and Lighting**

Pedestrian Pathway



# MURA 5 YEAR PROJECT SUMMARY AND CAPITAL IMPROVEMENT PLAN

The Capital Improvement Plan (CIP) serves as a general guiding document that identifies public investments that the Agency anticipates making over the upcoming 5 year period. It is intended to be a flexible planning document that is reviewed and updated on an annual basis in response to revenues received, changes to current identified projects, or to incorporate newly identified projects and improvements. The projects identified within the CIP do not include improvements that are funded through Owner Participation Agreements, wherein the developer funds the initial improvement and the Agency reimburses the developer for identified public improvements through increased tax revenues generated by the developer's project.

**Legacy Crossing District Capital Improvement Plan 2020-2025**

Community Infrastructure Projects											
Project Name	Project Description	Project Cost	Agency Contribution	Construction Year	Status	2020	2021	2022	2023	2024	2025
<b>Street Projects</b>											
Almon and First Street Repaving	Reconstruction of Almon Street between 3rd Street and A Street and First between Almon and Jackson	\$ 400,000	\$ 150,000	2020	Committed	\$ 150,000					
Almon Asbury Alley Paving Project	Paving and drainage for alley between Sixth and Third Street	\$ 85,000	\$ 60,000	2020	Committed	\$ 60,000					
First Street Paving	Curbing and paving of First Street between Asbury and Lieuallen	\$ 110,725	\$ 55,363	2022	Planned			\$ 55,363			
Fifth Street Paving	Cubing, sidewalks, paving and storm drainage Fourth Street, Lilly to Asbury	\$ 62,150	\$ 31,075	2022	Planned			\$ 31,075			
Henley Street Paving	Cubing, sidewalks, paving and storm drainage on Henley	\$ 161,250	\$ 80,625	2023	Planned				\$ 80,625		
Lieuallen Street Paving	Cubing, sidewalks, paving and storm drainage for Lieuallen, A to HWY 8	\$ 267,500	\$ 133,750	2024/2025	Planned					\$ 25,000	\$ 133,750
Lieuallen /Third/HWY 8 Intersection	Reconstruction and realignment of intersection to improve sight distance, merging issues, turning movements, and general traffic flow.	\$ 275,000	\$ 137,500	2024/2025	Planned					\$ 25,000	\$ 137,500
District Pavement Improvements		Varies	Varies	TBD	Planned						
<b>Water Projects</b>											
A Street Water Main Replacement	A Street water main replacement Almon to Asbury	\$ 60,000	\$ 30,000	2020	Committed	\$ 30,000					
A Street Water Main Replacement	A Street water main replacement Asbury to Lieuallen	\$ 145,000	\$ 72,500	2023	Planned				\$ 72,500		
District Fire Hydrant Replacement	Replacement of fire hydrants in excess of 50 years old	Varies	Varies	TBD	Planned		\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000
<b>Sanitary Sewer Projects</b>											
Sanitary Sewer Manhole Replacements	Replacement of aged brick or block sewer manholes with new precast manholes to reduce amount of infiltration and inflow	Varies	Varies	Varies	Planned			\$ 25,000			
<b>Community Infrastructure Projects Total</b>		<b>\$ 400,000</b>	<b>\$ 2,173,313</b>		Planned	<b>\$ 240,000</b>	<b>\$ 10,000</b>	<b>\$ 121,438</b>	<b>\$ 163,125</b>	<b>\$ 60,000</b>	<b>\$ 281,250</b>

Streetscape Enhancement Projects											
Project Name	Project Description	Project Cost	Agency Contribution	Construction Year	Status	2020	2021	2022	2023	2024	2025
A Street Frontage Improvement	50% contribution to Thompson Frontage Improvement on A Street	\$ 30,000	\$ 15,000	2020	Committed	\$ 15,000					
3rd Street Corridor Lighting & Sidewalk Improvements	Sidewalk reconstruction on Third Street between Lieuallen and Jackson Street	\$ 887,000	\$ 199,500	2020/2021	Committed	\$ 112,500	\$ 87,000				
Sixth Street Corridor Lighting	Decorative lighting fixture installations on Sixth Street Deakin and Jackson Street	\$ 140,000	\$ 20,000	2021	Committed		\$ 16,000				
Downtown Streetscape Improvements	Work includes curbs, gutter, sidewalk, street, lighting and street furnishing improvements	\$ 4,750,000	\$ 3,000,000	2022/2023	Planned			\$ 3,000,000			
General Streetscape Improvements	General Streetscape enhancement projects within the District	\$ 500,000	Varies	Varies	Planned	\$ 15,000	\$ 100,000	\$ 15,000	\$ 15,000	\$ 15,000	\$ 25,000
<b>Streetscape Enhancement Projects Total</b>		<b>\$ 6,791,674</b>	<b>\$ 3,567,932</b>			<b>\$ 142,500</b>	<b>\$ 203,000</b>	<b>\$ 3,015,000</b>	<b>\$ 15,000</b>	<b>\$ 15,000</b>	<b>\$ 25,000</b>

Community Placemaking Projects											
Project Name	Project Description	Project Cost	Agency Contribution	Construction Year	Status	2020	2021	2022	2023	2024	2025
North Main Beautification Project	Landscaping and lighting improvements near A and Main	\$ 150,000	\$ 50,000	2020	Planned	\$ 50,000					
Ghormley Park Facility Improvements	Reconstruction of deteriorated tennis courts for pickle ball	\$ 240,000	\$ 120,000	2021	Committed		\$ 125,000				
South Couplet Beautification Project	Streetscape and landscape enhancements per the 2015 City Beautification Plan	\$ 125,000	\$ 125,000	2023	Planned				\$ 100,000		
Public Art Installation	Public Art installations in various locations	Varies	Varies	Various	Committed	\$ 20,000		\$ 25,000		\$ 25,000	
<b>Community Placemaking Projects Total</b>		<b>\$ 75,000</b>	<b>\$ 75,000</b>			<b>\$ 70,000</b>	<b>\$ 125,000</b>	<b>\$ 25,000</b>	<b>\$ 100,000</b>	<b>\$ 25,000</b>	<b>\$ -</b>

Special Projects											
Project Name	Project Description	Project Cost	Agency Contribution	Construction Year	Status	2020	2021	2022	2023	2024	2025
Downtown Streetscape Plan	Development of design plan for the repair and replacement of deteriorating downtown public infrastructure in the Legacy Crossing District	\$ 120,000	\$ 60,000	2020	Committed	\$ 60,000					
Sixth and Jackson Property Development	Hello Walk construction at Sixth and Jackson Property	\$ 185,000	\$ 185,000	2020	Committed		\$ 185,000				
First to Lieuallen Pathway Project	Construction of pedestrian/bicycle pathway along First Street alignment between Almon and Lieuallen Street to Crossing at Highway 8 Phase II Lilly to Lieuallen	\$ 68,500	\$ 68,500	2022	Planned			\$ 68,500			
South Main Underpass Construction	Construction of pedestrian underpass of South Main at Paradise Creek	\$ 650,000	\$ 200,000	2023	Planned				\$ 250,000		
Pedestrian and Bicycle Improvements	Development and construction of various pedestrian and bicycle pathways, facilities and lighting	Varies	Varies	Varies	Planned			\$ 80,000			
<b>Special Projects Total</b>		<b>\$ 1,816,346</b>	<b>\$ 631,996</b>			<b>\$ 60,000</b>	<b>\$ 185,000</b>	<b>\$ 148,500</b>	<b>\$ 250,000</b>	<b>\$ -</b>	<b>\$ -</b>

Annual Investments											
Projected District Investment Portfolio				2020	2021	2022	2023	2024	2025		
Community Infrastructure Projects		\$	\$ 534,563	\$ 240,000	\$ 10,000	\$ 121,438	\$ 163,125	\$ 60,000	\$ 281,250		
Streetscape Enhancement Projects		\$	\$ 3,390,500	\$ 142,500	\$ 203,000	\$ 3,015,000	\$ 15,000	\$ 15,000	\$ 25,000		
Community Placemaking Projects		\$	\$ 320,000	\$ 70,000	\$ 125,000	\$ 25,000	\$ 100,000	\$ 25,000	\$ -		
Special Projects		\$	\$ 702,146	\$ 60,000	\$ 185,000	\$ 148,500	\$ 250,000	\$ -	\$ -		
<b>Total</b>		\$	\$ 4,947,209	\$ 512,500	\$ 523,000	\$ 3,309,938	\$ 528,125	\$ 100,000	\$ 306,250		

Legacy Ending Fund Balance		2020	2021	2022	2023	2024	2025
		\$291,126	\$213,212	\$427,688	\$81,932	\$173,264	\$67,491

